

Site Plan

For

Bank of America Rhode Island Ave. ADA Renovation



915 RHODE ISLAND AVENUE NE
WASHINGTON, DC 20018

APPLICANT
RAPID PERMIT SERVICE, INC.
7711 GARRISON ROAD, 2ND FLOOR
LANDOVER HILLS, MD 20784

LUIS GARCIA
(301) 731-4767

ARCHITECT
LITTLE DIVERSIFIED ARCHITECTURAL CONSULTING
4245 NORTH FAIRFAX DRIVE, SUITE 650
ARLINGTON, VA 22203

DESIREE COOPER
(703) 908-4538

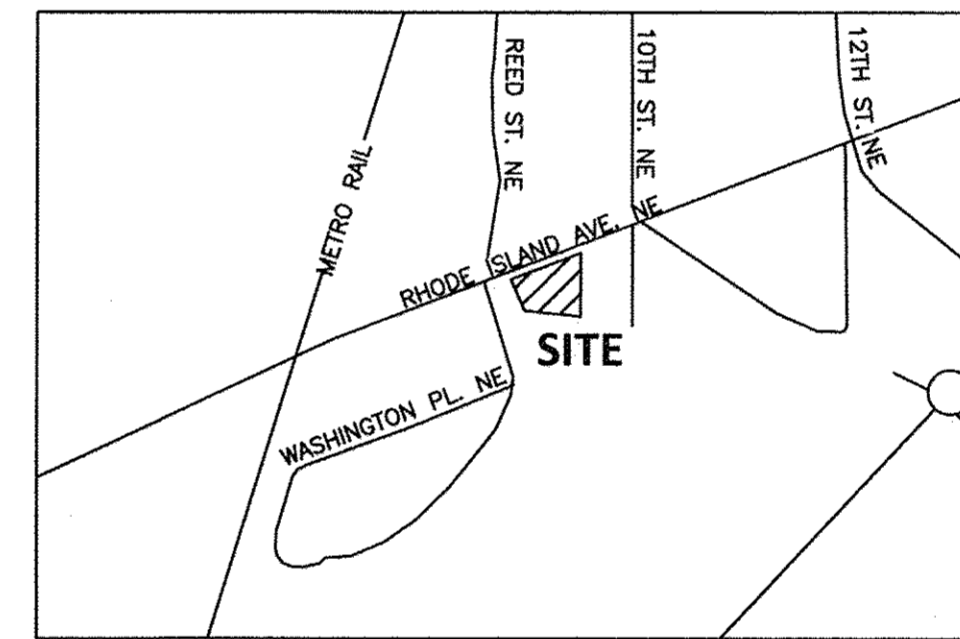
OWNER REPRESENTATIVE
LINCOLN HARRIS CSG
3401 COLUMBIA PIKE, SUITE 302
ARLINGTON, VA 22204

KEVIN BURNS
(202) 255-3439



CIVIL ENGINEER
LITTLE DIVERSIFIED
ARCHITECTURAL CONSULTING
4245 N. FAIRFAX DRIVE, SUITE 650
ARLINGTON, VA 22203

MALAK BAHRAMI
(571) 257-2865
malak.bahrani@littleonline.com



VICINITY MAP
N.T.S.

02

SHEET INDEX	
C0.00	COVER SHEET
C1.00	EXISTING CONDITIONS
C1.00A	PHASING PLAN & PROJECT NARRATIVE
C1.01	DEMOLITION PLAN
C1.02	LAYOUT
C1.03	GRADING PLAN
C1.04	PHASE I TRAFFIC CONTROL PLAN
C1.05	PHASE II TRAFFIC CONTROL PLAN
C1.05	TRAFFIC CONTROL PLAN DETAILS
C2.00	SITE DETAILS
C2.01	SITE DETAILS

02

GENERAL NOTES

- NO WORK IN PUBLIC SPACE BEFORE 9:30AM OR AFTER 3:30PM, WEEKDAYS AND 7AM-7PM ON SATURDAYS; ANY WORK AFTER 7PM WILL REQUIRE A SEPARATE DCRA AFTER HOURS PERMIT. PROPER TRAFFIC CONTROL SIGNS AND DEVICES FOR ROADWAY AND SIDEWALK CLOSURES MUST BE SET UP AT ALL TIMES DURING CONSTRUCTION. MUST HAVE ALL PERMITS ONSITE (EXCAVATION/OCCUPANCY/TCP). ENP SIGNS MUST BE PRE-PRINTED FROM D.TOPS WITH ALL APPROPRIATE PERMIT INFORMATION, CONTACT AND PERMIT NUMBERS. ENP SIGNS MUST BE POSTED AT LEAST 72 HOURS PRIOR TO OCCUPYING PUBLIC SPACE IN RESIDENTIAL AREA AND 24 HRS IN BUSINESS DISTRICT. ALL FLAGGERS ON SITE MUST BE CERTIFIED. ALL STEEL PLATES USED MUST BE PERMITTED ON SEPARATE OCCUPANCY PERMIT AND MUST BE PINNED, WELDED AND FEATHERED ACCORDING TO DC SPECIFICATIONS. A TRENCH SHALL NOT BE OPEN NO MORE THAN 120 FT AT A TIME, AFTER 120 FT YOU MUST START BACK FILLING. MUST CONTACT PSI INSPECTOR, OLUWASEUN OBADARE @ (202) 439-5045 PRIOR TO PERFORMING ANY RESTORATIONS.
- ALL OPERATIONS SHALL COMPLY WITH DISTRICT DEPARTMENT OF TRANSPORTATION STANDARD SPECIFICATIONS FOR HIGHWAYS AND STRUCTURES AND DESIGN AND ENGINEERING MANUAL 2014 DDOT GOLD BOOK. INSPECTOR TO BE PRESENT DURING WORK TIME, RESTORED TO EXISTING CONDITION. FOR THE TEMPORARY PATCHES SEE DDOT DWG. NO. 617.25. FOR THE PAVEMENT RESTORATION SEE DDOT DWG NO. 215.01 AND DDOT DWG. NO. 215.02. FOLLOW THE GUIDELINE PROVIDED IN OSHA'S OVERHEAD AND UNDERGROUND POWER LINE SAFETY. ALSO FOLLOW NFPA ELECTRICAL SAFETY AWARENESS NEC 2014.

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature

 Project Name: 20150122-00518

Bank of America - Rhode Island Ave. ADA
223-2844-00

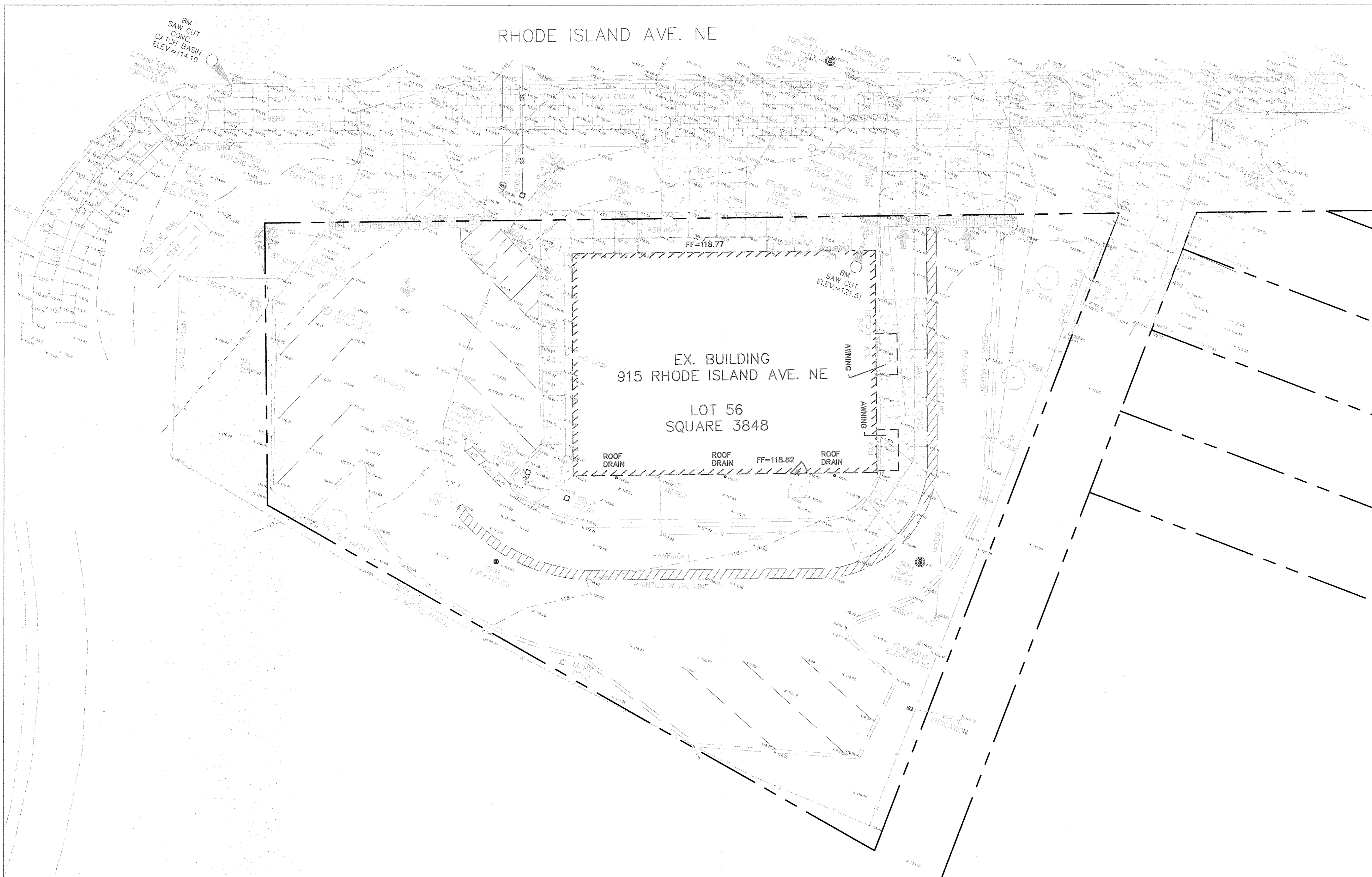
PROTOTYPE LAYOUT

CAD File Name

Description
COVER SHEET

Scale

C0.00



RHODE ISLAND AVE. NE

EX. BUILDING
915 RHODE ISLAND AVE. NE

LOT 56
SQUARE 3848

1. EXISTING CONDITIONS PLAN

SCALE: 1" = 10'-0"

LEGEND

- TREES
- SEWER MANHOLE
- LIGHT POLE
- BENCH MARK
- SEWER CLEANOUT

THIS PLAN IS PROVIDED FOR INFORMATIONAL PURPOSES ONLY. THIS PLAN IS PREPARED BY OTHERS. LITTLE DIVERSIFIED ARCHITECTURAL CONSULTING IS NOT RESPONSIBLE FOR ANY ERRORS OR OMISSIONS FROM THIS SURVEY.

NOTE: PLEASE CONTACT MISS UTILITY @811 TO LOCATE AND MARK EXISTING 12" WRAPPED-STEEL GAS MAIN LOCATED SOUTH OF SOUTH CURB ON RHODE ISLAND AVENUE NE.

02

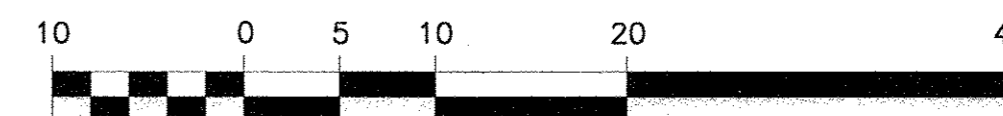


Know what's below.
Call before you dig.

CAUTION!!!

The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction

GRAPHIC SCALE



1" = 10'-0"

Dewberry
Consultants LLC
Formerly known as
Dewberry & Davis LLC

10003 DEREKWOOD LANE
SUITE 204
LANHAM, MD 20706
301.731.5551
301.731.0188 (FAX)
www.dewberry.com

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



476 North Carolina Street, Suite 650, Arlington, VA 22201
T: 703.908.4501 F: 703.908.4502

www.littleonline.com

This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature

Project Name

Bank of America - Rhode Island Ave. ADA
223-2844-00

PROTOTYPE LAYOUT

CAD File Name

Description

EXISTING CONDITIONS PLAN

Scale

C1.00

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:

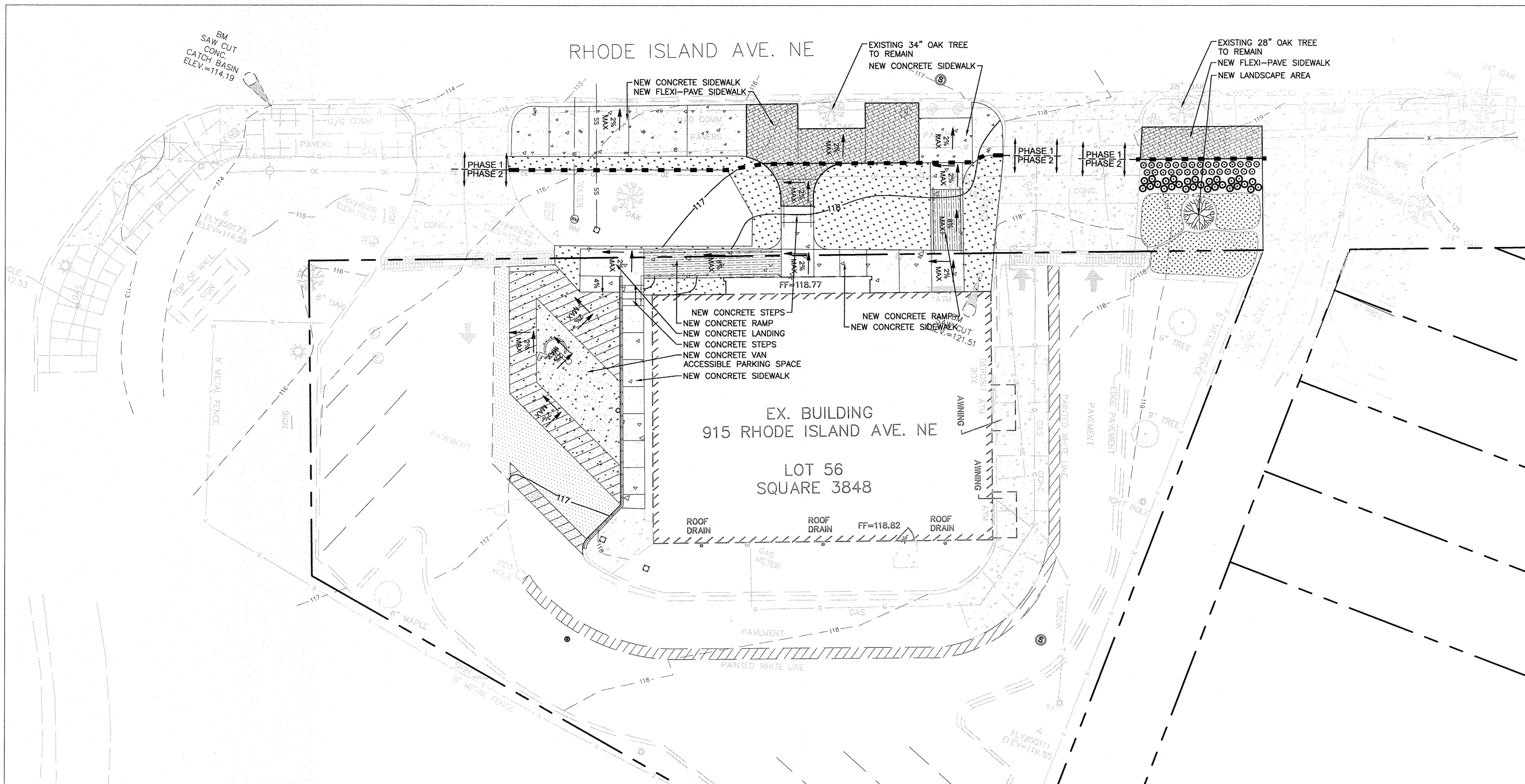


145 North Fairfax Drive, Suite 650, Arlington, VA 22203
T: 703.908.4501 F: 703.908.4502

www.littleonline.com

This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015



1A. PROJECT PHASING PLAN

SCALE: 1" = 10'-0"

GENERAL NOTES:

1. ALL TRAFFIC CONTROL SIGNAGE AND STRIPING TO CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
2. ALL PAVEMENT CUTS SHALL BE MADE BY SAW CUTS. EXISTING ELEVATIONS SHALL BE FIELD VERIFIED AND MATCHED.
3. CONTRACTOR TO COORDINATE ANY CHANGES IN FIELD CONDITIONS THAT MAY REVISE THE DESIGN WITH ARCHITECT/ENGINEER PRIOR TO PROCEEDING.
4. CONTRACTOR IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGES TO THE EXISTING LANDSCAPE, IRRIGATION, POWER AND/OR HARDSCAPE.

ACCESSIBILITY NOTE:

ADA REGULATIONS MANDATE A MAXIMUM OF 2% SLOPE IN ANY DIRECTION IN ACCESSIBLE PARKING SPACES AND ACCESS AISLES.

ADA REGULATIONS MANDATE REQUIRED EGRESS SIDEWALKS FROM PARKING TO THE BUILDING ENTRANCE AND PUBLIC RIGHT-OF-WAY CANNOT EXCEED 2% CROSS SLOPE AND 8.3% RUNNING SLOPE.

ALL GRADES IN THESE AREAS WILL BE VERIFIED BY ARCHITECT (USING A 2' DIGITAL LEVEL) PRIOR TO FINAL APPROVAL.

PROJECT NARRATIVE

THE PURPOSE OF THIS PROJECT IS TO REPAIR THE DAMAGED SIDEWALK IN FRONT OF THE BANK AND TO IMPROVE ACCESS TO THE BANK AND THE ATM BY BRINGING THE SIDEWALK UP TO CURRENT ADA STANDARDS. THE PROJECT WILL ALSO CREATE AN ACCESSIBLE PATH FROM THE PUBLIC SIDEWALK ADJACENT TO RHODE ISLAND AVE. NE TO THE BANK. THERE WILL BE TWO PHASES TO THE PROJECT.

PHASE I - THE PUBLIC SIDEWALK DIRECTLY ADJACENT TO RHODE ISLAND AVE. NE WILL BE RECONSTRUCTED WITH CONCRETE AND FLEXI-PAVE. THE CONTRACTOR TO CALL THE WARD 5 ARBORIST, SHAUN MCKIM ((202) 673-6813) & CAPITOL FLEXI-PAVE ((202) 760-1099) TO ENSURE THE STREET TREES ARE NOT IMPACTED DURING CONSTRUCTION.

PHASE II - THE REMAINING WORK WILL BE DONE ON SITE INCLUDING THE NEW SIDEWALKS, RAMPS, STEPS AND ADA PARKING SPACES. THE PUBLIC SIDEWALK WILL REMAIN OPEN AND CLEAR OF ANY OBSTRUCTIONS. THE CONTRACTOR SHALL PROVIDE UNOBSTRUCTED ACCESS TO THE BANK AT ALL TIMES. ALSO IN THIS PHASE, NEW LANDSCAPING WILL BE ADDED TO THE PUBLIC SPACE ADJACENT TO THE BANK EXIT LANES.



Know what's below.
Call before you dig.

CAUTION!!!

The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction

GRAPHIC SCALE



1" = 10'-0"

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature
M. A. Babson
DISTRICT OF COLUMBIA
No. PE 907181
PROFESSIONAL ENGINEER
01/22/2015

Project Name 20150122-00519

Bank of America - Rhode Island Ave. ADA
223-2844-00

PROTOTYPE LAYOUT

CAD File Name

Description

PROJECT PHASING PLAN & NARRATIVE

Scale

C1.00A

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. H. Johnson*
DISTRICT OF COLUMBIA
No. PE 907191
CIVIL PROFESSIONAL ENGINEER
01/22/2015

Project Name: 20150122-00520

Bank of America - Rhode Island Ave. ADA
223-2844-00
PROTOTYPE LAYOUT

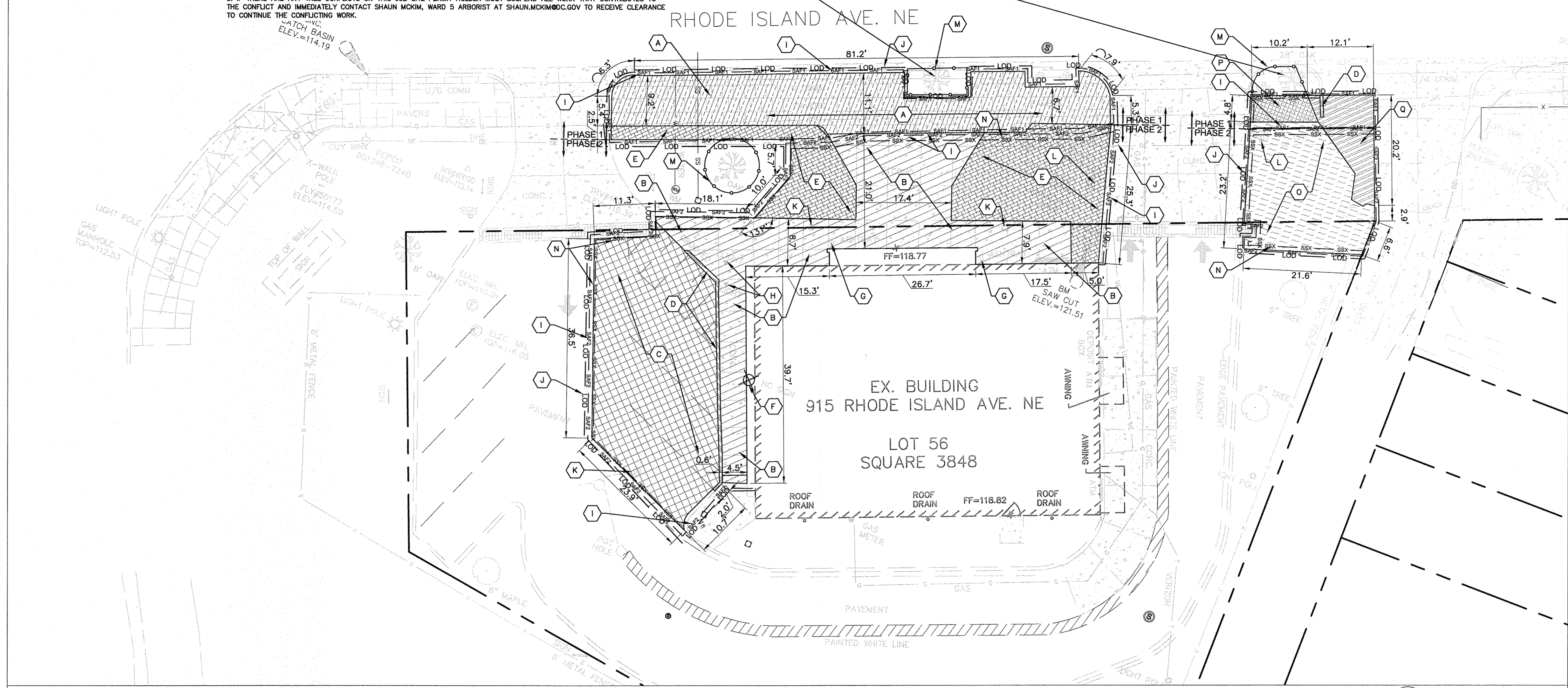
CAD File Name

Description: DEMOLITION PLAN

Scale

C1.01

- NOTE**
EXISTING 30.4" DBH & 28" DBH PIN OAKS MUST BE PROTECTED THROUGHOUT CONSTRUCTION.
- NO HEAVY EQUIPMENT SHALL BE USED TO REMOVE THE EXISTING HARDSCAPE WITHIN THE DRIP LINE OF THE PIN OAK. EXCAVATIONS WITHIN THE DRIP LINE OF THE PIN OAK MUST BE PERFORMED BY USE OF HAND TOOLS.
 - NO TREE ROOTS GREATER THAN 2" IN DIAMETER ARE TO BE CUT WITHOUT UFA PERMISSION.
 - EXPOSED TREE ROOTS TWO (2) INCHES AND LARGER IN DIAMETER MUST BE WRAPPED IN BURLAP OR OTHER APPROVED MATERIAL AND KEPT MOIST AT ALL TIMES.
 - NO TRENCHING IS ALLOWED WITHIN THE PIN OAK'S DRIP LINE DUE TO THE TREE ROOTS UNDERNEATH THE SIDEWALK.
 - IF THERE ARE ANY TREE CONFLICTS ON THIS JOB SITE PERMIT HOLDER MUST SUSPEND ALL WORK THAT CONTRIBUTES TO THE CONFLICT AND IMMEDIATELY CONTACT SHAUN MCKIM, WARD 5 ARBORIST AT SHAUN.MCKIM@DC.GOV TO RECEIVE CLEARANCE TO CONTINUE THE CONFLICTING WORK.



2. DEMOLITION PLAN

SCALE: 1" = 10'-0"

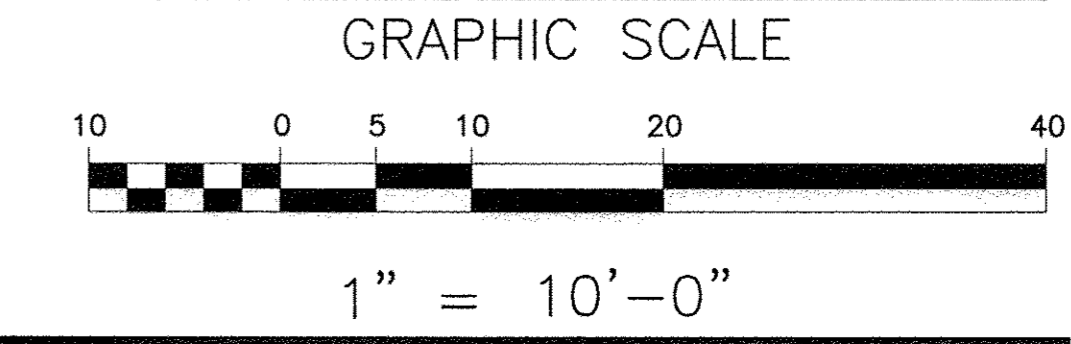
- GENERAL NOTES:**
- ALL TRAFFIC CONTROL SIGNAGE AND STRIPING TO CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
 - ALL PAVEMENT CUTS SHALL BE MADE BY SAW CUTS. EXISTING ELEVATIONS SHALL BE FIELD VERIFIED AND MATCHED.
 - CONTRACTOR TO COORDINATE ANY CHANGES IN FIELD CONDITIONS THAT MAY REVISE THE DESIGN WITH ARCHITECT / ENGINEER PRIOR TO PROCEEDING.
 - CONTRACTOR IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGES TO THE EXISTING LANDSCAPE, IRRIGATION, POWER AND/OR HARDSCAPE.

- DEMOLITION NOTES:**
- | | | | |
|---|----------------------------------|---|--|
| A. REMOVE EXISTING CONCRETE PAVER SIDEWALK | | K. CONTRACTOR TO ADJUST ALL CLEANOUTS, VALVE COVERS, MANHOLE RIMS, CATCH BASIN RIMS, METER COVERS, ETC. WITHIN LIMITS OF CONSTRUCTION TO MEET NEW ELEVATIONS. | |
| B. SAWCUT & REMOVE EXISTING CONCRETE SIDEWALK | | L. REMOVE SIGN, STOCKPILE FOR REUSE AND REPLACE AFTER GRADING. | |
| C. SAWCUT & REMOVE EXISTING ASPHALT PAVEMENT | | M. TREE PROTECTION - SEE DETAIL 1/C2.00 AND REFERENCE THE 2013 DDOT STANDARD SPECIFICATIONS FOR HIGHWAYS AND STRUCTURES SECTION 608.07 AND 608.08 | |
| D. SAWCUT & REMOVE CONCRETE CURB | | N. PROMPTLY AFTER DEMOLITION INSTALL SILT SOXX - SEE DETAIL 2/C2.00. | |
| E. REMOVE EXISTING LANDSCAPE AREA (FOR GRADING PURPOSES) | | O. PREPARE FOR NEW LANDSCAPE AREA: REMOVE TOP 4" OF SOIL/GRAVEL, SCARIFY SOIL TO 24" DEEP, REMOVE ANY REMAINING GRAVEL AND ADD 4" OF NEW TOP SOIL | |
| F. REMOVE EXISTING SIGN | | P. PREPARE EXISTING FLEXI-PAVE TO RECEIVE ADDITIONAL FLEXI-PAVE. COORDINATE WITH FLEXI-PAVE CONTRACTOR | |
| G. REMOVE EXISTING ASHTRAY | | Q. SAWCUT AND REMOVE PORTION OF EXISTING CONCRETE ALLEY APRON. | |
| H. REMOVE EXISTING LIGHTS IN SIDEWALK | | | |
| I. INSTALL SAFETY FENCE BEFORE DEMOLITION - SEE DETAIL 8/C2.01 | PHASE 1: SAF1
PHASE 2: SAF2 | | |
| J. LIMITS OF CONSTRUCTION
PHASE 1 DISTURBED AREA = 1,150 SF (0.03 AC)
PHASE 2 DISTURBED AREA = 3,050 SF (0.70 AC) | LOD | | |

NOTE: PLEASE CONTACT MISS UTILITY @811 TO LOCATE AND MARK EXISTING 12" WRAPPED-STEEL GAS MAIN LOCATED SOUTH OF SOUTH CURB ON RHODE ISLAND AVENUE NE.



CAUTION!!!
The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction



Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. J. Abraham*
DISTRICT OF COLUMBIA
No. PE 907181
PROFESSIONAL ENGINEER
01/22/2015

Project Name: 20150122-00528

Bank of America - Rhode Island Ave. ADA
223-2844-00

PROTOTYPE LAYOUT

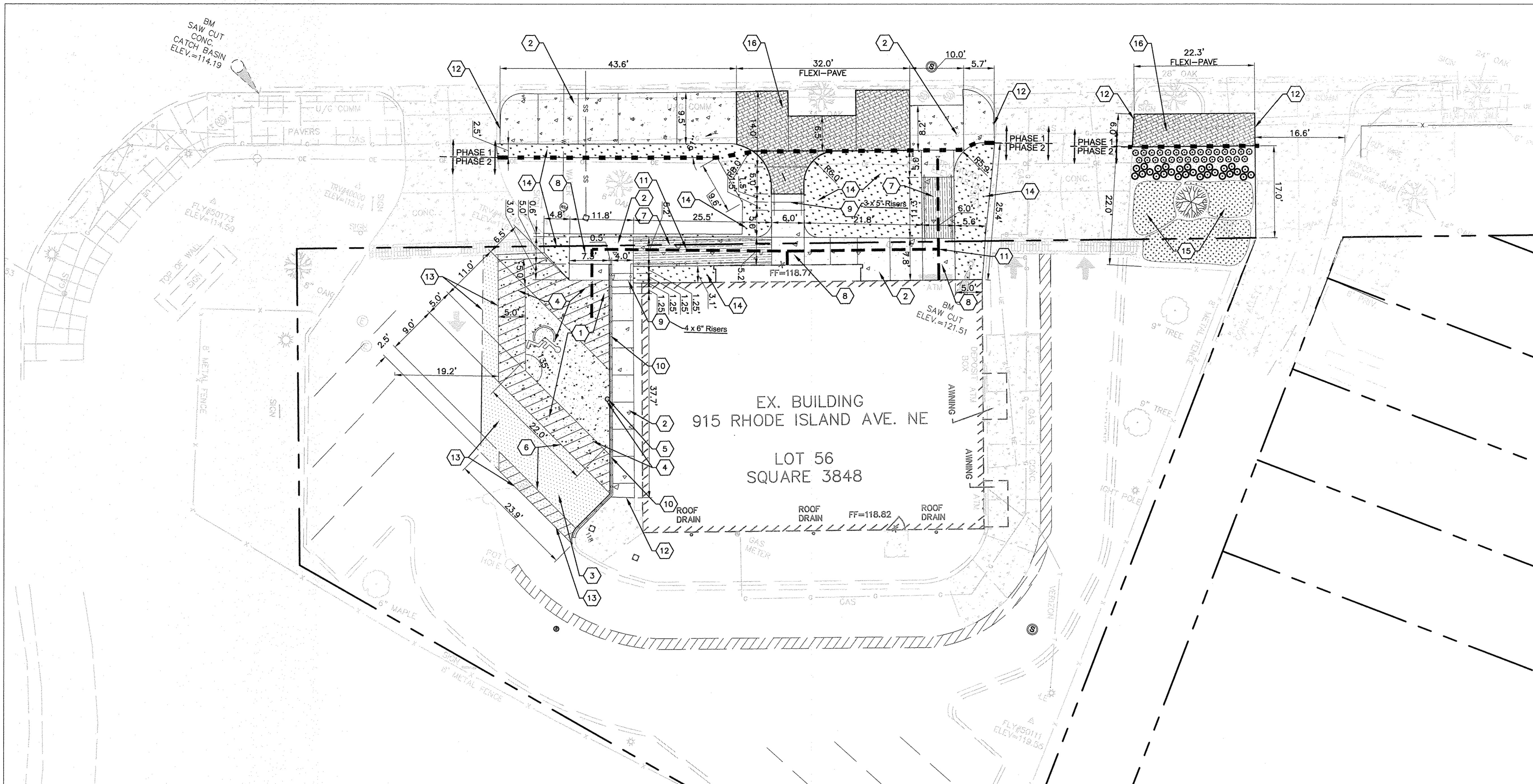
CAD File Name

Description

SITE LAYOUT PLAN

Scale

C1.02



3. LAYOUT PLAN

SCALE: 1" = 10'-0"

GENERAL NOTES:

- ALL TRAFFIC CONTROL SIGNAGE AND STRIPING TO CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
- ALL PAVEMENT CUTS SHALL BE MADE BY SAW CUTS. EXISTING ELEVATIONS SHALL BE FIELD VERIFIED AND MATCHED.
- CONTRACTOR TO COORDINATE ANY CHANGES IN FIELD CONDITIONS THAT MAY REVISE THE DESIGN WITH ARCHITECT/ENGINEER PRIOR TO PROCEEDING.
- CONTRACTOR IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGES TO THE EXISTING LANDSCAPE, IRRIGATION, POWER AND/OR HARDSCAPE.

ACCESSIBILITY NOTE:

ADA REGULATIONS MANDATE A MAXIMUM OF 2% SLOPE IN ANY DIRECTION IN ACCESSIBLE PARKING SPACES AND ACCESS AISLES.

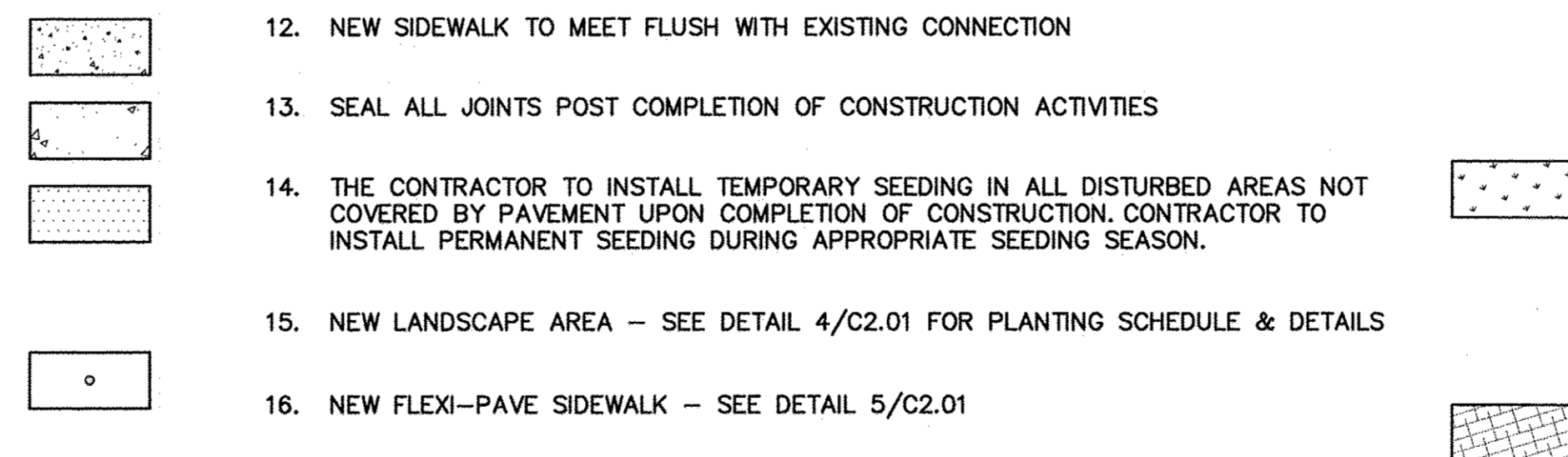
ADA REGULATIONS MANDATE REQUIRED EGRESS SIDEWALKS FROM PARKING TO THE BUILDING ENTRANCE AND PUBLIC RIGHT-OF-WAY CANNOT EXCEED 2% CROSS SLOPE AND 8.3% RUNNING SLOPE.

ALL GRADES IN THESE AREAS WILL BE VERIFIED BY ARCHITECT (USING A 2' DIGITAL LEVEL) PRIOR TO FINAL APPROVAL.

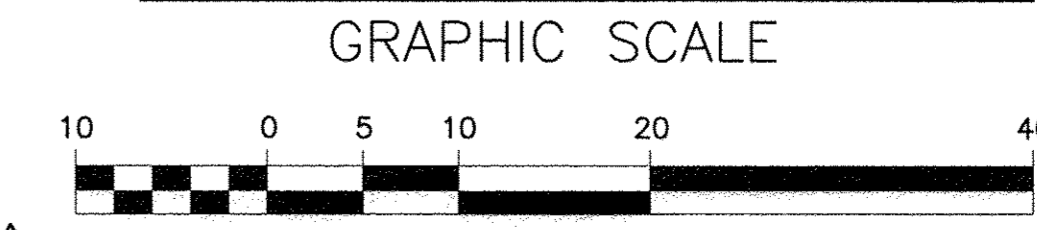
SITE LAYOUT NOTES:

- NEW HEAVY DUTY CONCRETE - SEE DETAIL 3/C2.00
- NEW CONCRETE SIDEWALK - SEE DETAIL 5/C2.00
- NEW ASPHALT PAVEMENT - SEE DETAIL 6/C2.00
- ACCESSIBLE STRIPING AND SIGNAGE - SEE DETAIL 8/C2.00
- NEW ACCESSIBLE SIGNAGE IN BOLLARD - SEE DETAIL 7 & 8/C2.00
- 4" WIDE WHITE PARKING STRIPING
- NEW RAMP WITH HAND RAIL - SEE DETAIL 1/C2.01
- ACCESSIBLE LANDING. MAXIMUM SLOPE TO BE 2% IN EACH DIRECTION. LANDSCAPE ARCHITECT WILL INSPECT THIS AREA WITH A 2' DIGITAL LEVEL TO ENSURE COMPLIANCE PRIOR TO FINAL APPROVAL - SEE DETAIL 2/C2.01.
- NEW CONCRETE STEPS WITH HAND RAIL - SEE DETAIL 3/C2.01
- NEW TURN DOWN CURB WITH HAND RAIL TO MEET EX. CURB - SEE DETAIL 9 & 10/C2.00
- ACCESSIBLE ROUTE - CONTRACTOR TO ENSURE THAT RUNNING SLOPE IS NOT GREATER THAN 5% AND THAT THE CROSS SLOPE IS NOT MORE THAN 2% ALONG THIS ROUTE; WHERE ROUTE EXCEEDS 5%, INSTALL HANDRAIL AS SHOWN.

- NEW SIDEWALK TO MEET FLUSH WITH EXISTING CONNECTION
- SEAL ALL JOINTS POST COMPLETION OF CONSTRUCTION ACTIVITIES
- THE CONTRACTOR TO INSTALL TEMPORARY SEEDING IN ALL DISTURBED AREAS NOT COVERED BY PAVEMENT UPON COMPLETION OF CONSTRUCTION. CONTRACTOR TO INSTALL PERMANENT SEEDING DURING APPROPRIATE SEEDING SEASON.
- NEW LANDSCAPE AREA - SEE DETAIL 4/C2.01 FOR PLANTING SCHEDULE & DETAILS
- NEW FLEXI-PAVE SIDEWALK - SEE DETAIL 5/C2.01



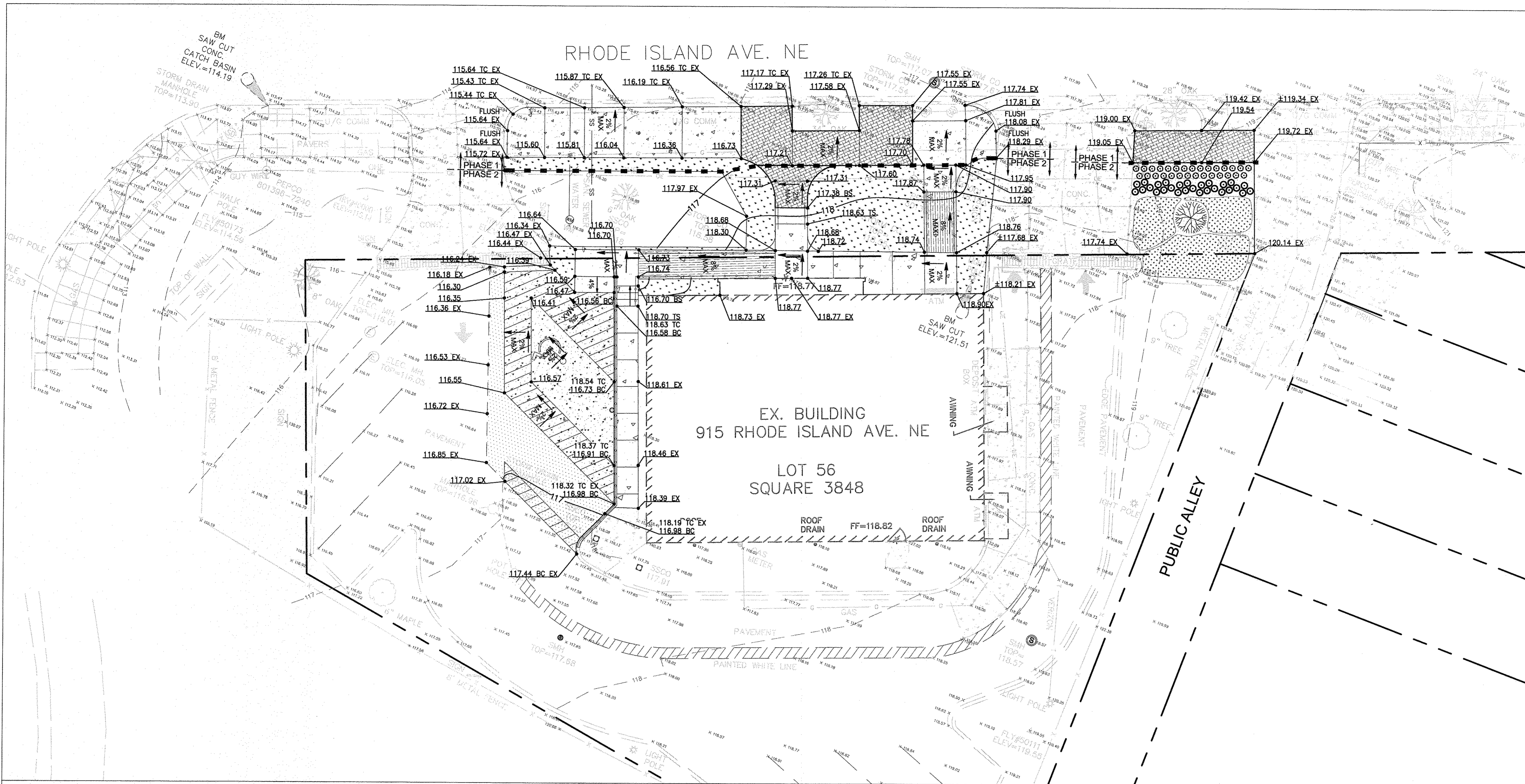
CAUTION!!!
The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction



NOTE: PLEASE CONTACT MISS UTILITY @811 TO LOCATE AND MARK EXISTING 12" WRAPPED-STEEL GAS MAIN LOCATED SOUTH OF SOUTH CURB ON RHODE ISLAND AVENUE NE.

02

1" = 10'-0"



4. GRADING PLAN

SCALE: 1" = 10'-0"

GENERAL NOTES:

- ALL TRAFFIC CONTROL SIGNAGE AND STRIPING TO CONFORM TO THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (M.U.T.C.D.).
- ALL PAVING CUTS SHALL BE MADE BY SAW CUTS. EXISTING ELEVATIONS SHALL BE FIELD VERIFIED AND MATCHED.
- CONTRACTOR TO COORDINATE ANY CHANGES IN FIELD CONDITIONS THAT MAY REVISE THE DESIGN WITH ARCHITECT/ENGINEER PRIOR TO PROCEEDING.
- CONTRACTOR IS RESPONSIBLE TO REPLACE AND/OR REPAIR ANY DAMAGES TO THE EXISTING LANDSCAPE, IRRIGATION, POWER AND/OR HARDSCAPE.

ACCESSIBILITY NOTE:

ADA REGULATIONS MANDATE A MAXIMUM OF 2% SLOPE IN ANY DIRECTION IN ACCESSIBLE PARKING SPACES AND ACCESS AISLES.

ADA REGULATIONS MANDATE REQUIRED EGRESS SIDEWALKS FROM PARKING TO THE BUILDING ENTRANCE AND PUBLIC RIGHT-OF-WAY CANNOT EXCEED 2% CROSS SLOPE AND 8.3% RUNNING SLOPE.

ALL GRADES IN THESE AREAS WILL BE VERIFIED BY ARCHITECT (USING A 2' DIGITAL LEVEL) PRIOR TO FINAL APPROVAL.

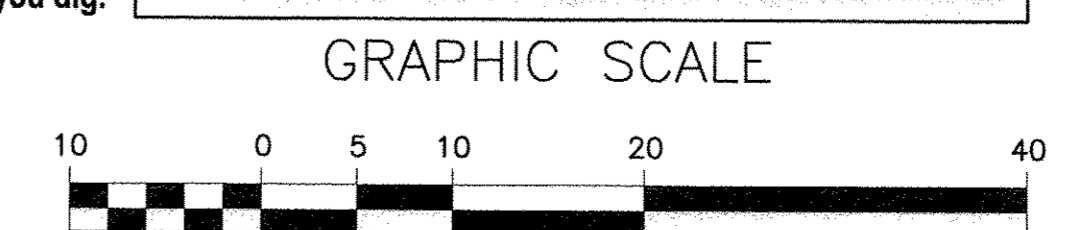
GRADING NOTES:

- VERIFY ALL EXISTING GRADES. REPORT ANY DISCREPANCIES TO ARCHITECT.
- ALL NEW CONTOURS AND SPOT ELEVATIONS ARE SHOWN AS FINISHED GRADES. THE GRADING CONTRACTOR MUST MAKE ALLOWANCES FOR THICKNESS OF PAVING SECTIONS, CONCRETE SLABS, ETC. WHERE APPLICABLE.
- THE ENTIRE SITE WITHIN THE LIMIT OF CONSTRUCTION SHALL BE GRADED TO DRAIN FREELY. ALL UNPAVED AREAS DISTURBED BY CONSTRUCTION, WHETHER INSIDE OR OUTSIDE OF THE LIMIT OF CONSTRUCTION, SHALL BE FINE GRADED AND SEEDED TO ESTABLISH A PERMANENT LAWN.
- CONTRACTOR SHALL PROTECT ALL EXISTING UTILITIES (INCLUDING SANITARY, STORM DRAINAGE, AND WATER) FROM DAMAGE DURING CONSTRUCTION. ALL UNDERGROUND UTILITIES EXPOSED DURING CONSTRUCTION OR THOSE LEFT WITH LESS THAN ACCEPTABLE MINIMUM EARTH COVER SHALL BE RELOCATED AS DETERMINED BY THE ARCHITECT.
- NO SLOPES SHALL BE GRADED STEEPER THAN 3:1, UNLESS INDICATED OTHERWISE.
- ANY GRADING BEYOND THE PROPERTY LINE INDICATED ON THE CONSTRUCTION DOCUMENTS IS A VIOLATION AND IS SUBJECT TO A FINE.
- APPROVAL OF THIS PLAN IS NOT AN AUTHORIZATION TO GRADE ADJACENT PROPERTIES. WHEN FIELD CONDITIONS WARRANT OFF-SITE GRADING, PERMISSION MUST BE OBTAINED FROM THE AFFECTED PROPERTY OWNERS.
- THE UTILITIES AND THE LOCATION THEREOF, SHOWN ON THE DRAWINGS, REPRESENT THE DESIGNERS UNDERSTANDING OF EXISTING UTILITIES IN THE CONSTRUCTION AREA. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION, DEPTH AND EXISTENCE OF ALL UTILITIES (ELECTRICAL, MECHANICAL, WATER, TELEPHONE, GAS ETC.) WITHIN THE CONSTRUCTION AREA WITH THE OWNER AND/OR THE APPROPRIATE UTILITY COMPANY PRIOR TO ANY EXCAVATION. THE OMISSION OF OR THE INCLUSION OF UTILITY LOCATIONS ON THE PLANS IS NOT TO BE CONSIDERED AS THE NONEXISTENCE OF OR A DEFINITE LOCATION. UTILITIES SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE.
- CONTRACTOR TO COORDINATE ANY CHANGES IN FIELD CONDITIONS THAT MAY REVISE THE DESIGN WITH ARCHITECT / ENGINEER PRIOR TO PROCEEDING.
- ANY UNSUITABLE MATERIAL ON SITE IS TO BE QUANTIFIED BY A GEOTECHNICAL ENGINEER, PRIOR TO REMOVING. CONTRACTOR MUST NOTIFY OWNER, OR OWNER'S REPRESENTATIVE IN CASE UNSUITABLE MATERIAL IS UNCOVERED.

NOTE: PLEASE CONTACT MISS UTILITY @811 TO LOCATE AND MARK EXISTING 12" WRAPPED-STEEL GAS MAIN LOCATED SOUTH OF SOUTH CURB ON RHODE ISLAND AVENUE NE.



CAUTION!!!
The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction



1" = 10'-0"

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and its design shown are the property of Little, Diversified Architectural Consulting. The reproduction, copying or other use of this drawing, without their written consent is prohibited and any infringement will be subject to legal action.

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. H. Bahrom*
Professional Engineer
No. PE 907181
01/22/2015

Project Name: 20150122-00522

Bank of America - Rhode Island Ave. ADA
223-2844-00

PROTOTYPE LAYOUT

CAD File Name:

Description:
GRADING PLAN

Scale:

C1.03

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. A. Bahram*
DISTRICT OF COLUMBIA
No. PE 807181
PROFESSIONAL ENGINEER
01/22/15

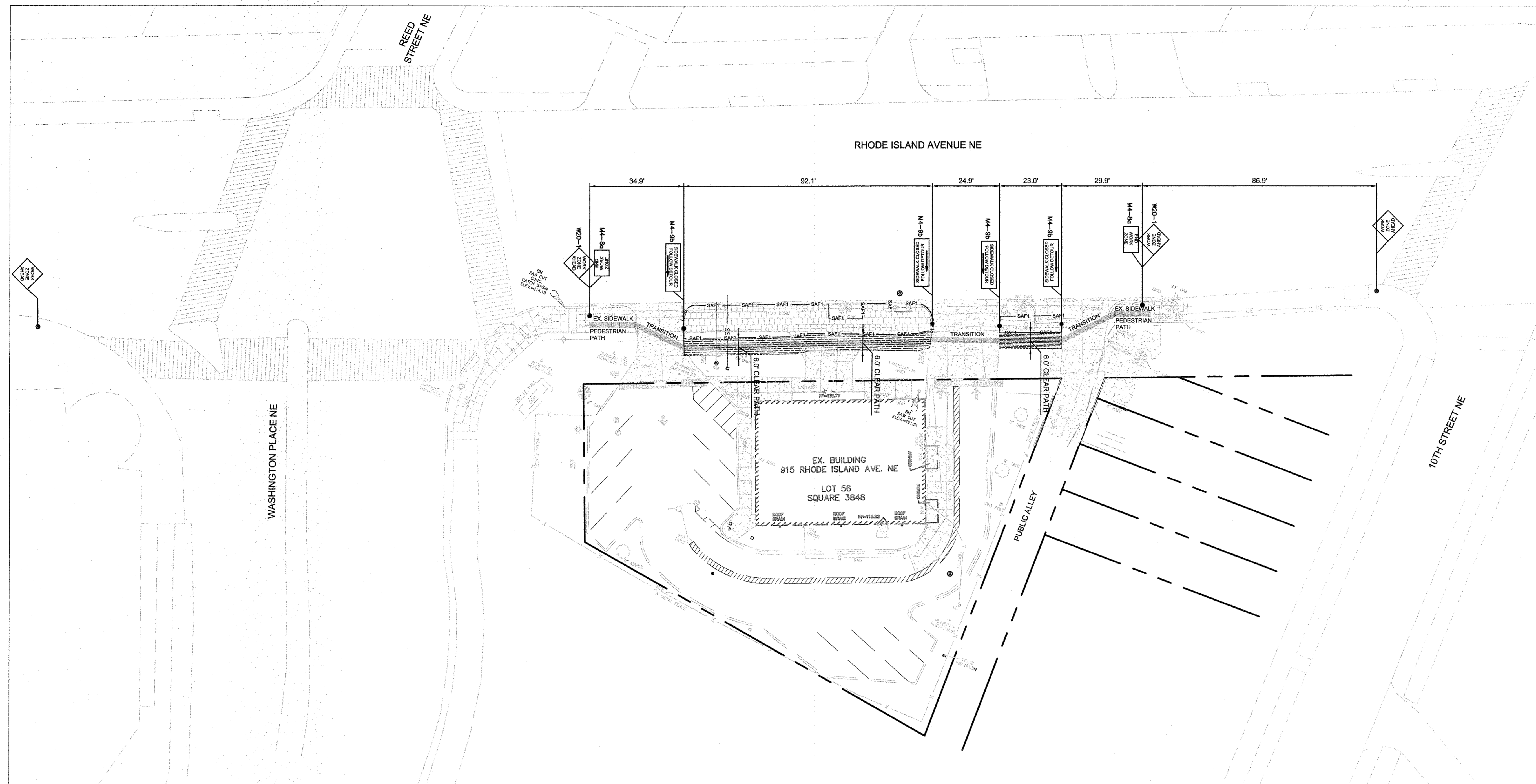
Project Name: 20150122-00523
Bank of America - Rhode Island Ave. ADA
223-2844-00
PROTOTYPE LAYOUT

CAD File Name:

Description: PHASE I TRAFFIC CONTROL PLAN

Scale:

C1.04



5. PHASE I TRAFFIC CONTROL PLAN

SCALE: 1" = 20'-0"

GENERAL NOTES:

- ALL ACTIVITIES SHALL BE CONDUCTED IN ACCORDANCE WITH THIS PLAN, UPON APPROVAL, AND THE D.C. TEMPORARY TRAFFIC CONTROL MANUAL 2006 EDITION AND PEDESTRIAN SAFETY AND WORK ZONE STANDARDS - COVERED AND OPEN WALKWAYS, JULY 2010.
- SIGN PANELS ARE SHOWN FACING THE DIRECTION OF THE VIEW OF THE PEDESTRIANS AND/OR MOTORISTS.
- ALL TRAFFIC CONTROL MEASURES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION RELATED ACTIVITIES AND REMOVED PROMPTLY UPON COMPLETION OF CONSTRUCTION ACTIVITIES.
- CONTRACTOR MUST ENSURE INSTALLED SIGNS ARE NOT IN CONFLICT WITH EXISTING SIGNAGE ON SITE AND SHALL NOTIFY THE ENGINEER OF ANY SUCH CONFLICTS.
- ALL SIGNAGE SYSTEMS, AND RELATED MATERIAL USED, MUST BE VISIBLE, LEGIBLE AND OPERATING IN GOOD CONDITION AS INTENDED AND DESIGNED.
- CONTRACTOR SHALL KEEP COPIES OF APPROVED PLANS ON SITE, AT ALL TIMES THROUGHOUT THE DURATION OF THE PROJECT, AND PRIOR TO COMMENCEMENT OF ANY AND ALL CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROL MEASURES AND SYSTEMS MUST BE MAINTAINED AT ALL TIMES THROUGHOUT THE DURATION OF THE PROJECT AND ONLY REMOVED UPON PROJECT COMPLETION.
- ALL CONSTRUCTION ACTIVITIES SHALL BE DURING THE HOURS OF 9:00 AM AND 3:00 PM.
- ALL VEHICLES/EQUIPMENT ENTERING AND EXITING THE CONSTRUCTION SITE SHALL BE CLEANED, AS NECESSARY, AS TO NOT SPREAD SEDIMENTS, SEDIMENT LADEN RUNOFF, DIRT, AND DEBRIS BEYOND THE LIMITS OF CONSTRUCTION.
- CONTRACTOR SHALL PROMPTLY CLEAN ANY DIRT/DEBRIS SPREAD AS A RESULT OF THE CONSTRUCTION ACTIVITIES
- CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY PERMITS NEED FOR PERFORMING ACTIVITIES THAT WILL ENCROACH INTO THE PUBLIC SPACE AND BEYOND LIMITS OF DISTURBANCE AS DELINEATED IN SHEET C1.01.

NOTES

- CONTRACTOR SHALL USE SAFETY FENCE TO OUTLINE THE PHASE I PROJECT LIMITS AS SHOWN.
- THE TEMPORARY PEDESTRIAN PATHWAY SHALL BE MAINTAINED TO NOT HAVE ANY ABRUPT ELEVATION VARIATIONS AND SHALL BE MAINTAINED TO BE FREE OF ANY DIRT, DEBRIS, SNOW, ICE OR ANY OTHER TRIPPING HAZARD.
- THE TEMPORARY PATHWAY SHALL BE INSPECTED AT A MINIMUM OF TWICE A DAY.
- THE TEMPORARY PATHWAY SHALL NOT HAVE ANY CROSS SLOPES GREATER THAN 1:48 EXCEPT AREA WHERE THE PATHWAY ABUTS EXISTING WALKWAYS WITH SLOPES GREATER THAN 1:48. THE LAST 6' CAN BE WARPED TO RECEIVE THE NON COMPLIANT SLOPE.

NOTE: SEE TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES ON C1.06

DURATION:

MARCH/ APRIL 2015 DEPENDENT ON WEATHER (15 DAYS WEATHER PERMITTING)

PROJECT DESCRIPTION:

THE PURPOSE OF THIS PROJECT IS TO REPAIR & REPLACE DAMAGE PUBLIC SIDEWALK AND TO IMPROVE ACCESS TO THE BANK AND THE ATM BY BRINGING THE SIDEWALK UP TO CURRENT ADA STANDARDS. DURING THE COARSE OF CONSTRUCTION (15 DAYS WEATHER PERMITTING) PEDESTRIAN WILL BE DIRECTED TO USE AN ALTERNATIVE PATH.

PHASE I OF THE PROJECT WILL ENTAIL THE RECONSTRUCTION OF A PORTION OF THE PUBLIC SIDEWALK ADJACENT TO RHODE ISLAND AVE. NE. PEDESTRIAN TRAFFIC WILL BE ROUTED TO THE SOUTH OF THE EXISTING SIDEWALK. THE CONTRACTOR WILL PROVIDE SAFE AND UNOBSTRUCTED ACCESS TO THE BANK AT ALL TIMES DURING CONSTRUCTION.

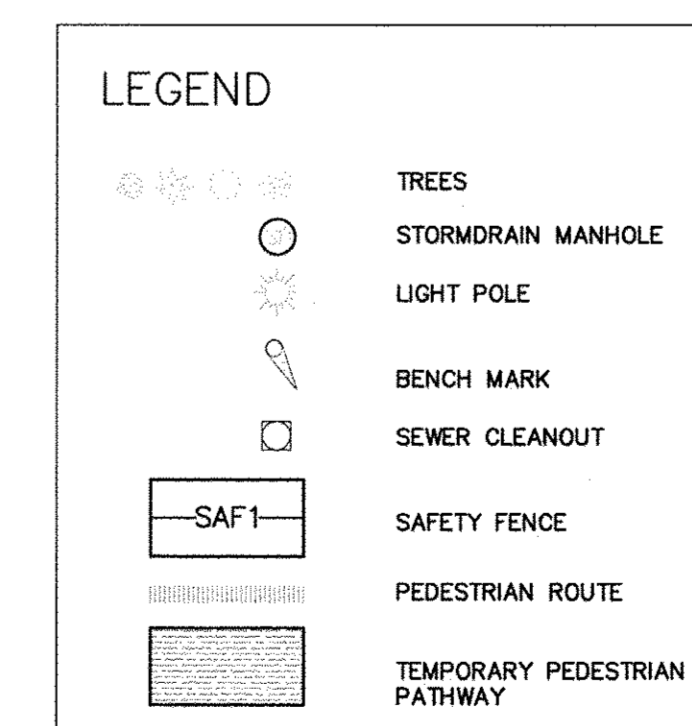
UPON APPROVAL THIS PLAN WILL BE IMPLEMENTED AND REMAIN IN EFFECT FOR A DURATION OF 15 DAYS, WEATHER PERMITTING. ALL TRAFFIC CONTROL MEASURES SHALL REMAIN IN PLACE IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES GENERAL NOTES, SEE BELOW.

ALL CONSTRUCTION ACTIVITIES AND MATERIAL STORAGE SHALL BE WITH THE LIMITS OF CONSTRUCTION SHOWN ON SHEET C1.01. NO VEHICULAR TRAVEL LANES ON RHODE ISLAND AVENUE WILL BE AFFECTED AS A RESULT OF THIS PLAN.

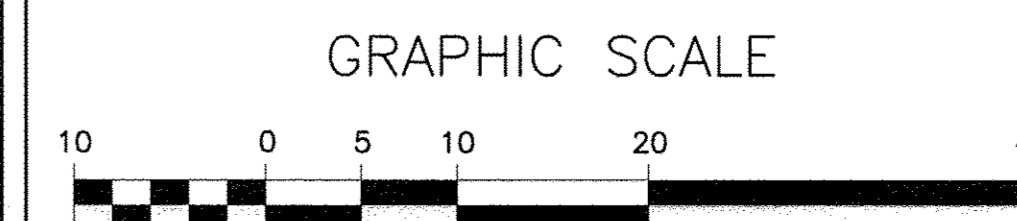
PHASE I NARRATIVE

- CONTRACTOR SHALL CONSTRUCTION A TEMPORARY PEDESTRIAN PATHWAY.
- UPON COMPLETION OF THE ALTERNATE PEDESTRIAN PATHWAY CONTRACTOR SHALL SETUP THE SIGNAGE SHOWN.
- INSTALL SAFETY FENCE FOR PHASE I, SEE SHEET C1.01.
- REMOVE 2 FOOT WIDE STRIP OF THE EXISTING AND INSTALL PERIMETER SEDIMENT CONTROL AS PER SHEET C1.01
- COMPLETE DEMOLITION ACTIVITIES AND COMMENCE CONSTRUCTION
- UPON COMPLETION OF PHASE I CONSTRUCTION ACTIVITIES REMOVE SAFETY FENCE AND PHASE I SIGNAGE.
- REMOVE TEMPORARY PEDESTRIAN PATHWAY AND PROCEED TO PHASE 2.

CAUTION!!!
The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction



Know what's below.
Call before you dig.



1" = 10'-0"

Bank of America Rhode Island Ave. ADA Renovation

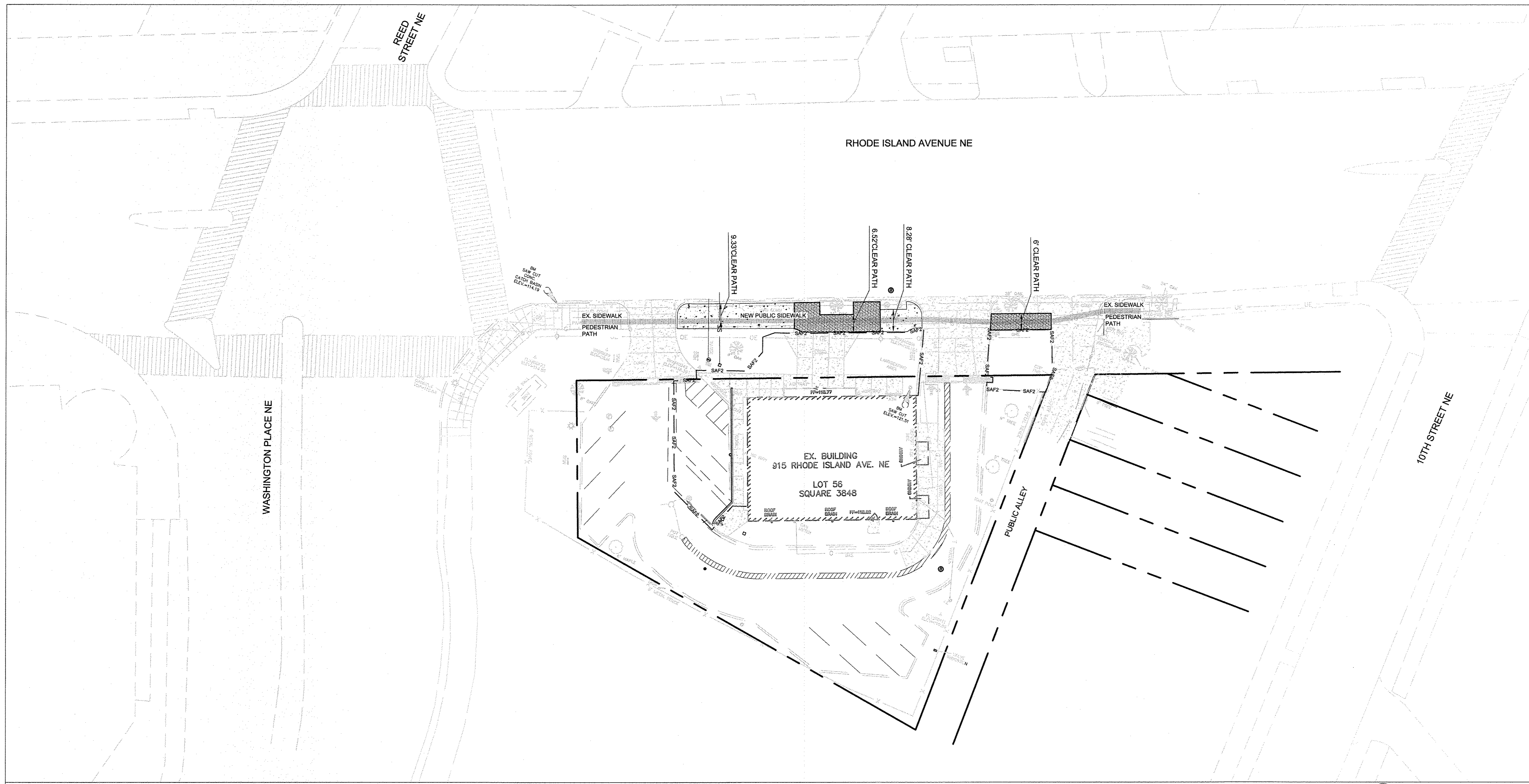
915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015



6. PHASE I TRAFFIC CONTROL PLAN

SCALE: 1" = 20'-0"

GENERAL NOTES:

- ALL ACTIVITIES SHALL BE CONDUCTED IN ACCORDANCE WITH THIS PLAN, UPON APPROVAL, AND THE D.C. TEMPORARY TRAFFIC CONTROL MANUAL 2006 EDITION AND PEDESTRIAN SAFETY AND WORK ZONE STANDARDS - COVERED AND OPEN WALKWAYS, JULY 2010.
- SIGN PANELS ARE SHOWN FACING THE DIRECTION OF THE VIEW OF THE PEDESTRIANS AND/OR MOTORISTS.
- ALL TRAFFIC CONTROL MEASURES SHALL BE INSTALLED AND INSPECTED PRIOR TO ANY CONSTRUCTION RELATED ACTIVITIES AND REMOVED PROMPTLY UPON COMPLETION OF CONSTRUCTION ACTIVITIES.
- CONTRACTOR MUST ENSURE INSTALLED SIGNS ARE NOT IN CONFLICT WITH EXISTING SIGNAGE ON SITE AND SHALL NOTIFY THE ENGINEER OF ANY SUCH CONFLICTS.
- ALL SIGNAGE SYSTEMS, AND RELATED MATERIAL USED, MUST BE VISIBLE, LEGIBLE AND OPERATING IN GOOD CONDITION AS INTENDED AND DESIGNED.
- CONTRACTOR SHALL KEEP COPIES OF APPROVED PLANS ON SITE, AT ALL TIMES THROUGHOUT THE DURATION OF THE PROJECT, AND PRIOR TO COMMENCEMENT OF ANY AND ALL CONSTRUCTION ACTIVITIES.
- ALL TRAFFIC CONTROL MEASURES AND SYSTEMS MUST BE MAINTAINED AT ALL TIMES THROUGHOUT THE DURATION OF THE PROJECT AND ONLY REMOVED UPON PROJECT COMPLETION.
- ALL CONSTRUCTION ACTIVITIES SHALL BE DURING THE HOURS OF 9:00 AM AND 3:00 PM.
- ALL VEHICLES/EQUIPMENT ENTERING AND EXITING THE CONSTRUCTION SITE SHALL BE CLEANED, AS NECESSARY, AS TO NOT SPREAD SEDIMENTS, SEDIMENT LADEN RUNOFF, DIRT, AND DEBRIS BEYOND THE LIMITS OF CONSTRUCTION.
- CONTRACTOR SHALL PROMPTLY CLEAN ANY DIRT/DEBRIS SPREAD AS A RESULT OF THE CONSTRUCTION ACTIVITIES
- CONTRACTOR SHALL BE RESPONSIBLE FOR ACQUIRING ANY PERMITS NEEDED FOR PERFORMING ACTIVITIES THAT WILL ENCROACH INTO THE PUBLIC SPACE AND BEYOND LIMITS OF DISTURBANCE AS DELINEATED IN SHEET C1.01.

NOTES

- CONTRACTOR SHALL USE SAFETY FENCE TO OUTLINE THE PHASE I PROJECT LIMITS AS SHOWN.
- THE TEMPORARY PEDESTRIAN PATHWAY SHALL BE MAINTAINED TO NOT HAVE ANY ABRUPT ELEVATION VARIATIONS AND SHALL BE MAINTAINED TO BE FREE OF ANY DIRT, DEBRIS, SNOW, ICE OR ANY OTHER TRIPPING HAZARD.
- THE TEMPORARY PATHWAY SHALL BE INSPECTED AT A MINIMUM OF TWICE A DAY.
- THE TEMPORARY PATHWAY SHALL NOT HAVE ANY CROSS SLOPES GREATER THAN 1:48 EXCEPT AREA WHERE THE PATHWAY ABUTS EXISTING WALKWAYS WITH CROSS SLOPES GREATER THAN 1:48. THE LAST 6' CAN BE WARPED TO RECEIVE THE NON COMPLIANT SLOPE.

NOTE: SEE TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES ON C1.06

DURATION:

MARCH/ APRIL 2015 DEPENDENT ON WEATHER (15 DAYS WEATHER PERMITTING)

PROJECT DESCRIPTION:

THE PURPOSE OF THIS PROJECT IS TO IMPROVE ACCESS TO THE BANK AND THE ATM BY BRINGING THE SIDEWALK UP TO CURRENT ADA STANDARDS. DURING THE COARSE OF CONSTRUCTION (15 DAYS WEATHER PERMITTING) PEDESTRIAN WILL BE DIRECTED TO USE AN ALTERNATIVE PATH.

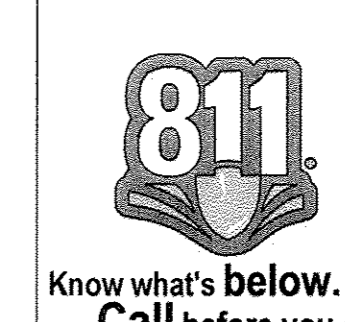
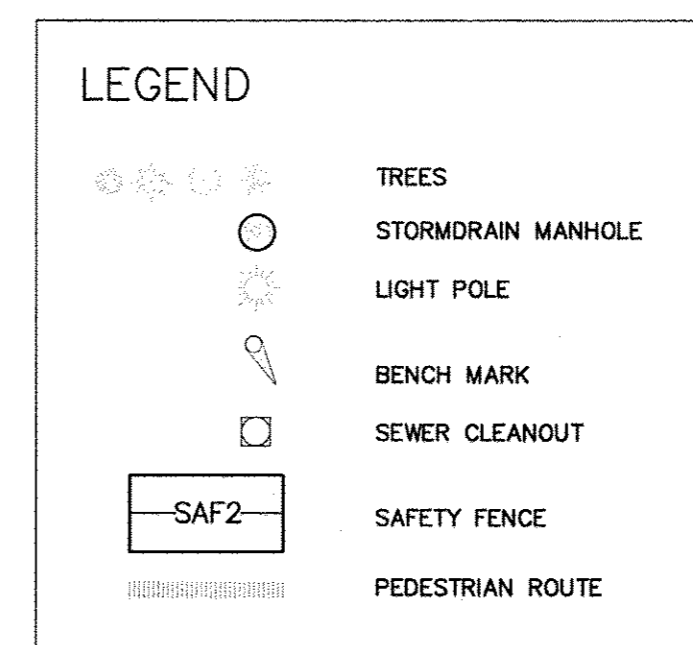
PHASE II OF THE PROJECT WILL ENTAIL THE RECONSTRUCTION OF THE ACCESSIBLE ROUTE FROM THE PUBLIC SIDEWALK AND PARKING LOT TO THE BANK ENTRANCE AS WELL AS THE ADA PARKING SPACES TO BECOME COMPLIANT WITH THE 2010 ADA STANDARDS. THE PEDESTRIAN ROUTE DURING PHASE II OF THE PROJECT IS NOT IMPACTED. THE NEW PUBLIC SIDEWALK ADJACENT TO RHODE ISLAND AVE. NE WILL BE OPEN AND A CLEAR PATH OF TRAVEL. THE CONTRACTOR WILL PROVIDE ACCESS TO THE BANK AT ALL TIMES DURING CONSTRUCTION.

UPON APPROVAL, THIS PLAN WILL BE IMPLEMENTED AND REMAIN IN EFFECT FOR A DURATION OF 15 DAYS, WEATHER PERMITTING. ALL TRAFFIC CONTROL MEASURES SHALL REMAIN IN PLACE IN ACCORDANCE WITH THE TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES GENERAL NOTES, SEE BELOW.

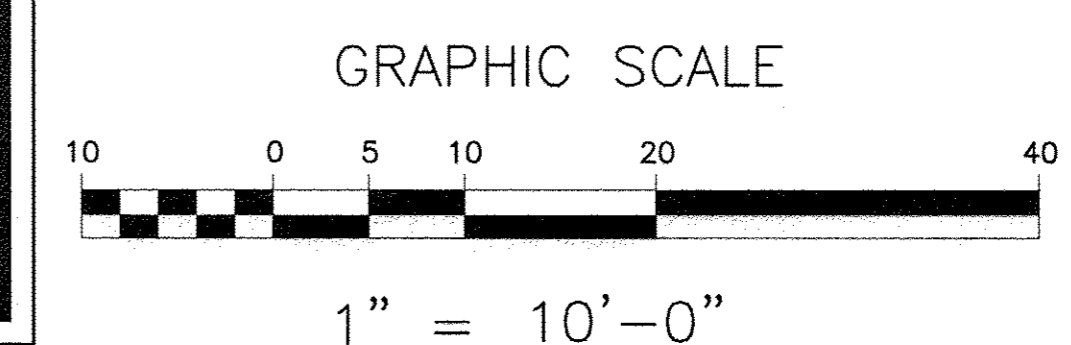
ALL CONSTRUCTION ACTIVITIES AND MATERIAL STORAGE SHALL BE WITH THE LIMITS OF CONSTRUCTION SHOWN ON SHEET C1.01. NO VEHICULAR TRAVEL LANES ON RHODE ISLAND AVENUE WILL BE AFFECTED AS A RESULT OF THIS PLAN.

PHASE II NARRATIVE

- CONTRACTOR SHALL ENSURE SAFE UNOBSTRUCTED PEDESTRIAN ACCESS TO THE BANK THROUGHOUT THE DURATION OF CONSTRUCTION
- CONTRACTOR SHALL INSTALL SAFETY FENCE AS DELINEATED ON SHEET C1.01.
- ALL CONSTRUCTION RELATED ACTIVITIES AND DELIVERIES SHALL BE CONTAINED ONSITE AND SHALL NOTE AFFECT ANY OF THE TRAVEL LANES ON RHODE ISLAND AVENUE NE.



CAUTION!!!
The locations and elevations of existing underground utilities as shown on this drawing are only APPROXIMATE. No guarantee is either expressed or implied as to the completeness of accuracy thereof. The contractor shall be exclusively responsible for determining the exact utility locations and elevations prior to the start of construction



Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B141121
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. J. Shram*
Professional Engineer Seal: DISTRICT OF COLUMBIA CIVIL ENGINEER No. PE 907181 01/22/2015

Project Name: 20150122-00524
Bank of America - Rhode Island Ave. ADA 223-2844-00
PROTOTYPE LAYOUT

CAD File Name:
Description: PHASE II TRAFFIC CONTROL PLAN

Scale:

C1.05

TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES

GOVERNMENT OF THE DISTRICT OF COLUMBIA
District Department of Transportation

Public Space Regulation Administration
PSRA



GENERAL
TRAFFIC CONTROL PLAN (TCP) SUBMITTAL GUIDELINES

12TH EDITION, May 21, 2012

This document is prepared to provide guidance for Traffic Control Plan submissions so they may be approved in a timely manner. Missing or insufficient information will delay approval.

APPROVAL: If a Traffic Control Plan (TCP) or Maintenance of Traffic (MOT) Plan is submitted as part of a Roadway or Bridge Reconstruction, Rehabilitation or Resurfacing Project to TOA, it will be reviewed by the Traffic/Civil Engineering Group of TOA Safety Team.

If a Traffic Control Plan (TCP) or Maintenance of Traffic (MOT) Plan is submitted as part of a public space application to PSRA/Public Space Regulation Administration, it will be reviewed by the Traffic/Civil Engineering Group of PSRA Technical Review Team

COORDINATION: DDOT expects that the submitter has coordinated with Public Space Regulation Administration and the TOA, IPMA on the amount of public space needed for a project to ensure coordination with adjacent projects, and special events appropriate detour routing, and adequate level of service for street operations.

REVIEW: TCPs shall be subject to review and verification by DDOT staff for conformance to submission requirements.

SUBMISSION REQUIREMENTS: The requirements for submission are based on the 2009 Manual on Uniform Traffic Control Devices (MUTCD), DC Temporary Traffic Control Manual Guidelines and Standards -2006 Edition DDOT, Traffic Services Administration, and the Design & Engineering Manual - DC Department of Transportation IPMA; Pedestrian Safety and Work Zone Standards, Pedestrian Safety and Work Zone Standards; Covered and Open Walkways; Preliminary Design Review Meeting Submittal Requirements Checklist. All items are commonly available to contractors and their engineers.

Standards

- Traffic Control Plan (TCP) must be tailored to fit contractor's specific situation.
- All traffic control shall conform to the standards set forth in 2009 Edition Manual on Uniform Traffic Control Devices (MUTCD) and DC Temporary Traffic Control Manual Guidelines and Standards -2006 Edition DDOT.

Conditions

- Cannot place equipment of any type: in a "NO PARKING ANYTIME ZONE", "NO STANDING OR PARKING ANYTIME", and "NO STANDING OR PARKING METRO BUS ZONE".
- All required dimensions shall be shown on crane and dumpster applications
- If any prohibiting signs (Regulatory) are proposed ("No Left Turn", "No Right Turn"; whether symbolic or text message), advance coordinate with the IPMA Traffic and Parking Operation engineers, must occur to ensure the adequate traffic movements are provided in the vicinity of the project site.
- When traffic signals are specified in the contract, the contractor will provide the timing plan for approval and will also provide whatever detection they feel will best fit the situation. District traffic is to be consulted to help review timing plans and detection zones. Overhead lighting must be provided at each signal location.
- Each plan page of all submissions must include the following statement. "I certify that this plan conforms to the requirements set forth in the 2009 Edition Manual on Uniform Traffic Control Devices (MUTCD), the 2006 DDOT DC Temporary Traffic Control Manual, Guidelines and Standards and adheres to DDOT Standards Specifications for Highways and Structures," followed by an original signature. This requirement is exempted for submission with Professional Engineer stamp.

TYPICAL TCPs FOR UTILITY WORK: Twenty-one typical TCPs for utility work only are available through the DDOT website in PDF format for use. Please visit the website for further information: http://ddot.dev.dc.gov/ddot/cwp/view_a.1250,a.640384.asp. The above mentioned is the link to the Temporary Traffic Control Manual Guidelines and Standards, Work Zone Pocket Guide, and Utility Work Zone Typical.

- When submitting DDOT typicals, make sure to specify street geometry, existing street dimensions, street names, etc. Each typical Traffic Control Plan drawing should contain the list of locations for which the TCP is applicable.
- Submit only those typicals that apply. The submitted typical TCPs should fit to the real street conditions. Otherwise, submit MOT/TCP individually by actual real street conditions and work zone location.

- All traffic control shall adhere to DDOT Standards Specifications for Highways and Structures [the "blue book"]. Reference section 104.02 Maintenance of Traffic, 614.02 PCC Barrier and 616.01 – 616.23 Traffic Control.
- Review MOT plans including: TCP drawings, Traffic Detour Plans, Sequence of Construction. Ensure that the submitted staging plan on the construction site provide maximum protection to motorists and pedestrians for ingress and egress.

Property Information

- Provide detail information about property location
- Provide Name, Address, Main Telephone Number, Emergency Telephone Number, /Cell Phone/, E-Mail Address of the person(s), the person responsible for the submission of the application and the attached MOT/TCP drawings.

Project Information

- Provide TOPS tracking number
- Provide Project Name
- Provide Address of Premise for which Public Space Work is proposed.
- Provide Lot(s) and Square(s) number.

MOT/TCP Requirements

- According to D.DOT requirements all Traffic Control Plan drawings shall be presented in engineering scale 1"= 20'. (When the scale is smaller than required, we are unable to correctly analyze the Traffic Control Plan).
- Specify the MUTCD number, and size of all temporary signs utilized on the MOT/TCP drawings.
- Specify North.
- Specify Street Names for all streets adjacent to Work Zone.
- Specify scope and sequence of work in narrative.
- Specify location and length of work (Advanced Warning Area, Transition Area, Buffer Area, Active Work Zone, and Termination Area).
- Specify duration of work.
- Specify schedule of work hours.
- Specify limits of work.
- Specify limits of project.
- Show advance warning area, where the advance warning signs shall be located at proper intervals to inform motorists of what to expect.
- Specify direction of travel, number of lanes, lane widths and posted speed limit.

- Provide all adjacent intersections of the street under construction or under utility work (independent of the area of the street under construction).
- Specify street geometry, median, curb and gutter lines, existing street dimensions, and orientations; existing location and width of Right-Of-Way for the street under construction or utility work and its intersecting streets.
- Specify if parking is to be restricted and if bus zone will need to be relocated.
- Specify bike lane(s), track and bus restrictions.
- Specify existing traffic control at the Intersections: stop control intersections or signal control intersections (for heavy urbanized area)
- Specify placement of all devices. [Arrow board panels, signs, cones, drums, attenuators, barricades, etc.]
- Specify spacing of devices. [Arrow board panels, signs, cones, drums, barricades, etc.]
- Specify taper and tangent lengths.
- Specify if turning radius will impact bus and trash truck turns.
- Orient traffic sign and device symbols with directions of traffic.
- Show work vehicle locations.
- Notes are encouraged.
- Show Key and/or Legend.
- Show existing pavement markings and proposed temporary pavement markings. Make distinction between them.
- Show existing traffic and parking operation signs including RPP signs, and proposed temporary WZ signs. Make distinction between them.
- A detour plan is required when the closing of traffic on main or intersecting roadway, for reconstruction or construction purposes. Ensure that detour plan are clearly identified with temporary guide detour signage which shall be accompanied with appropriate message sign indicating street name to eliminate confusion for motorists (Do not use abbreviation on the message sign).
- Provide safe access for pedestrians. All temporary pathways shall be clearly identified, wheelchair – usable, protected from motor vehicle traffic, and free of pedestrian hazards (holes, debris, dust, mud etc.). The pedestrians have to have a safe access for crossing the intersections as well as passing sidewalks, during all phases and sub phases of construction.
- Incorporate sidewalks, and crosswalks. Show detour for pedestrian traffic and provide appropriate pedestrians signage such as "Sidewalk Closed, Arrow, Use Other Side", "Sidewalk Closed", "Sidewalk Closed, Cross Here" etc.

Other Information

- Waiver for cutting on suspended streets must be approved by PSOA Permitting Office prior to MOT/TCP review by PSOA Traffic/ Civil Engineering Group, and Engineer Tech. of PSOA Technical Review Team.

PEDESTRIAN SAFETY AND WORK ZONE STANDARDS - COVERED AND OPEN WALKWAYS, JULY 2010

Open Walkways on the Sidewalk

- Construction Facing Side:** The open walkway must be separated from the adjacent work zone by a chain link fence or other similar barrier. Any gate in the fence must swing inward toward the work zone. The fence must be properly anchored but cannot be bolted or attached in any similar manner into the sidewalk. Light debris mesh shall be used when necessary. No light debris mesh shall be installed within 30 feet of the intersection of two roadways.
- Width:** Must maintain a minimum of 8 feet unobstructed sidewalk width in the Central Business District, 6 feet in all other areas.
- Modification Requests:** All standards noted above are subject to modification based on site-specific conditions. Requests for modification must be made in writing with the submission of the traffic control plan and must explain the exact modification requested and the site-specific conditions for requesting the modification. Once approved, no modifications are allowed without the prior written approval of the work zone technician that approved the traffic control plan. In cases where, in the professional judgment of the site superintendent or his or her designee, field modification is required to prevent an imminent threat to public health or safety, field modifications are allowed. In such cases the work zone technician that approved the traffic control plan must be notified of the change within 24 hours and a traffic control plan showing the change must be submitted to that work zone technician with 72 hours. The work zone technician must either approve or disapprove the request in writing. If the change is approved, then the new traffic control plan becomes a part of the permit already issued for the original traffic control plan. If the work zone technician determines that the change is not required, then the superintendent or his or her designee must return the work zone to its original approved condition.

SPECIAL EVENTS: Scheduled Special event such as sporting activities, parades, major concerts, or major conventions can have significant impacts on traffic operations within District of Columbia. Special events usually generate large volumes of pedestrian and vehicular traffic, and congestion generally occurs on the city large streets segment at or near the generator. Managing traffic during special events can result in reduced congestion and delay and improved safety.

A traffic control plan must be submitted by the event sponsor at least four weeks 20 working days in advance of the event for approval by the DDOT.

INSPECTION: DDOT reserves the right to periodically inspect work zones to ensure compliance, that safety measures are in place, and that the measures conform to the approved TCP and criteria listed on the Traffic Control Plan Inspection Criteria document.

The whole package of documentations which should include: PSOP Approved permit(s), together with Approved Maintenance of Traffic or Approved Traffic Control Plan, Traffic Control Plan Submittal Guidelines, Traffic Control Plan Inspection Criteria must be at the construction site during all phases and sub phases of construction until its 100% completion for mandatory inspection.

Our goal is a safe work zone, thank you for your cooperation!

DDOT / PSRA/ PLAN REVIEW DIVISION

d. *Serving with Integrity and Excellence*

**Bank of America
Rhode Island Ave.
ADA Renovation**

915 Rhode Island Aveune NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature

Project Name **20150122-00525**

Bank of America - Rhode Island Ave. ADA
223-2844-00

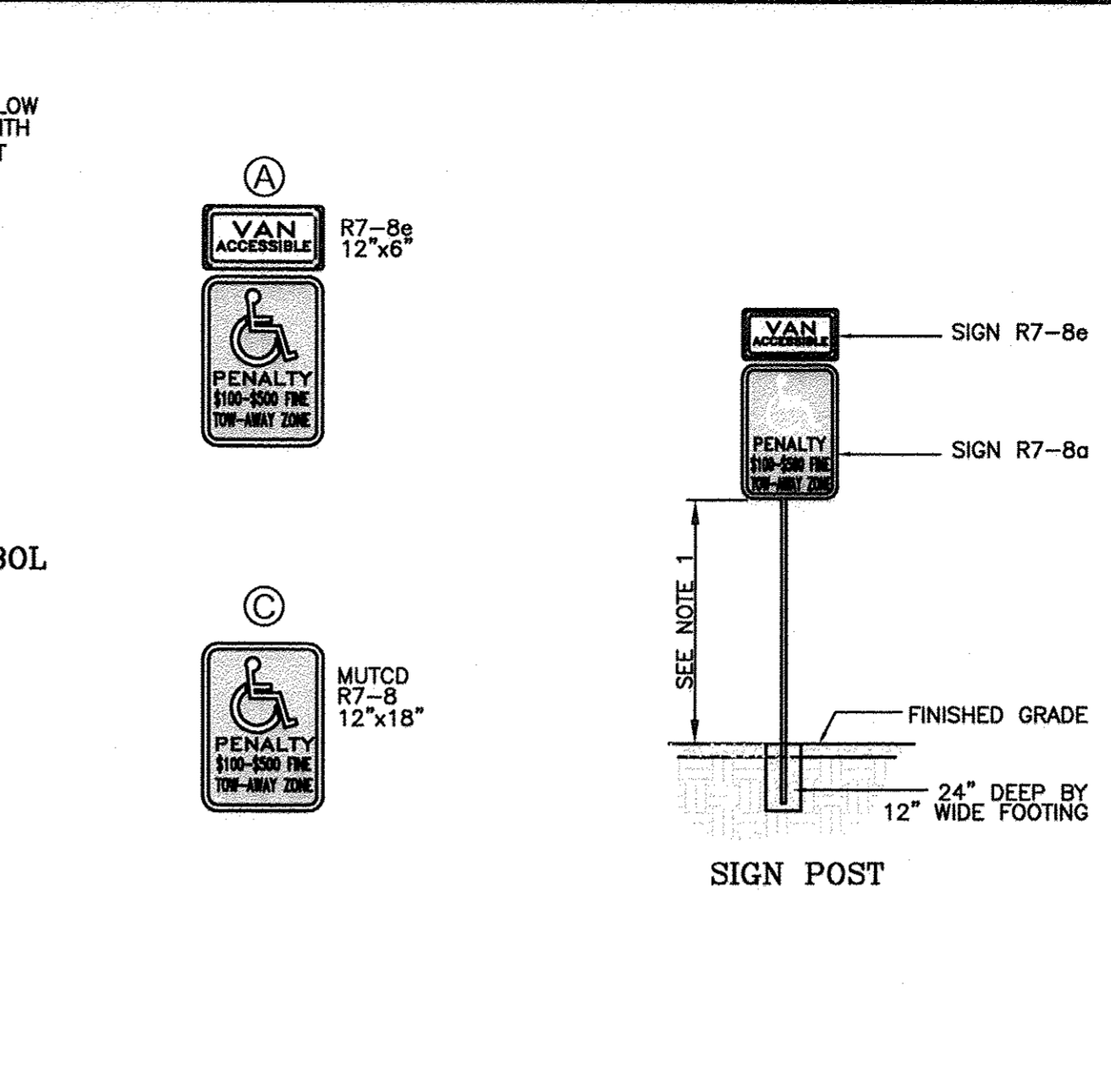
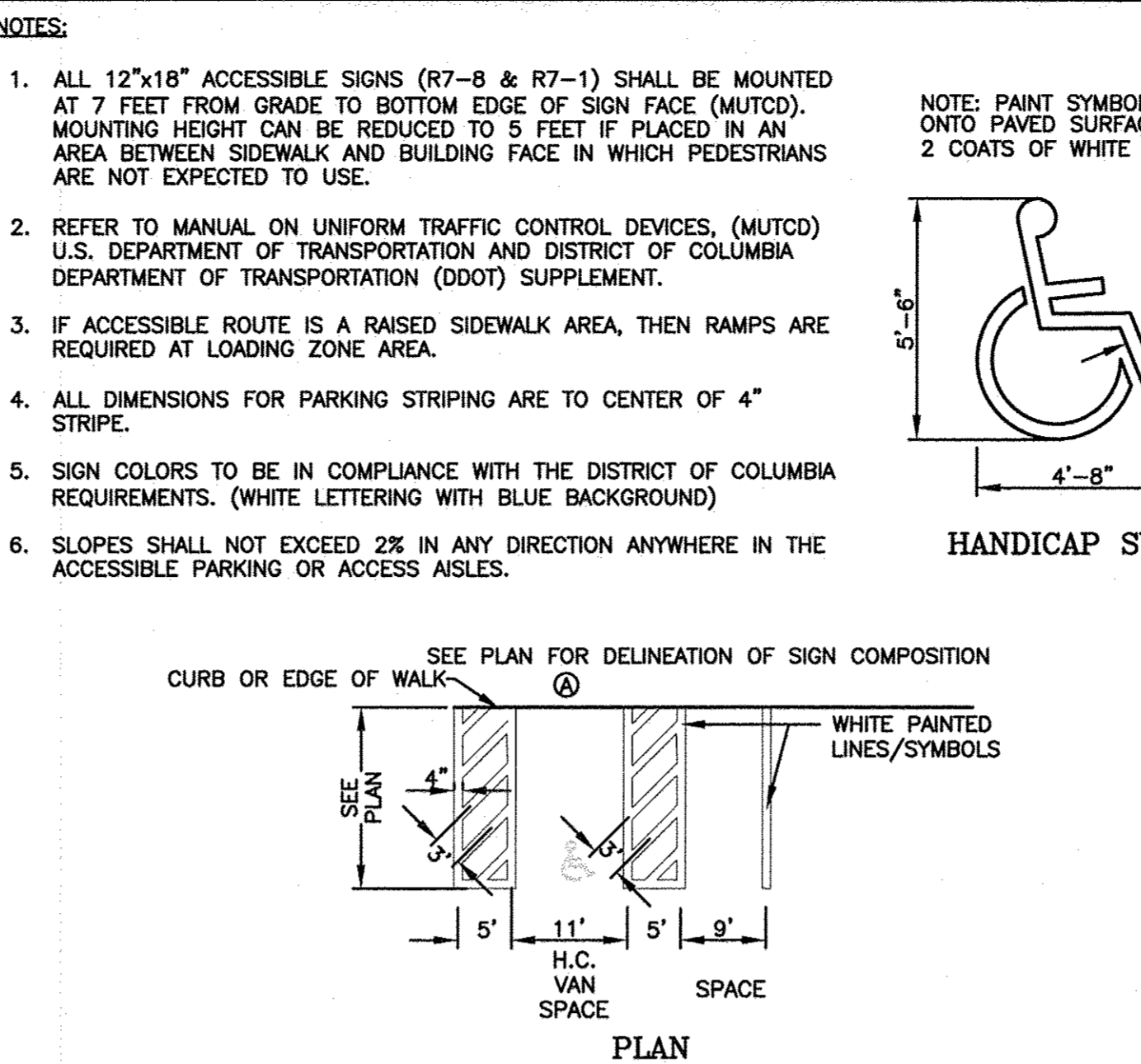
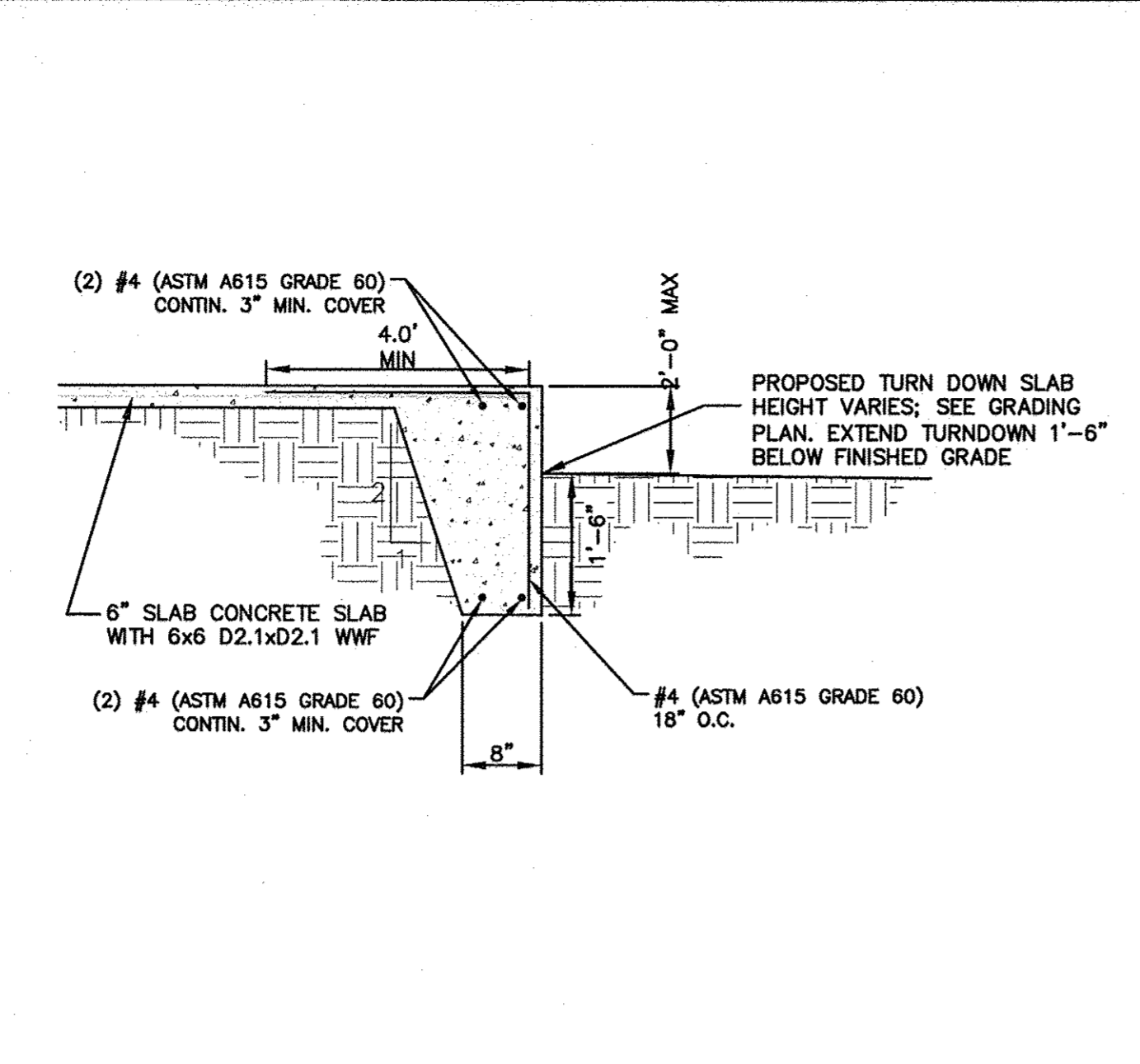
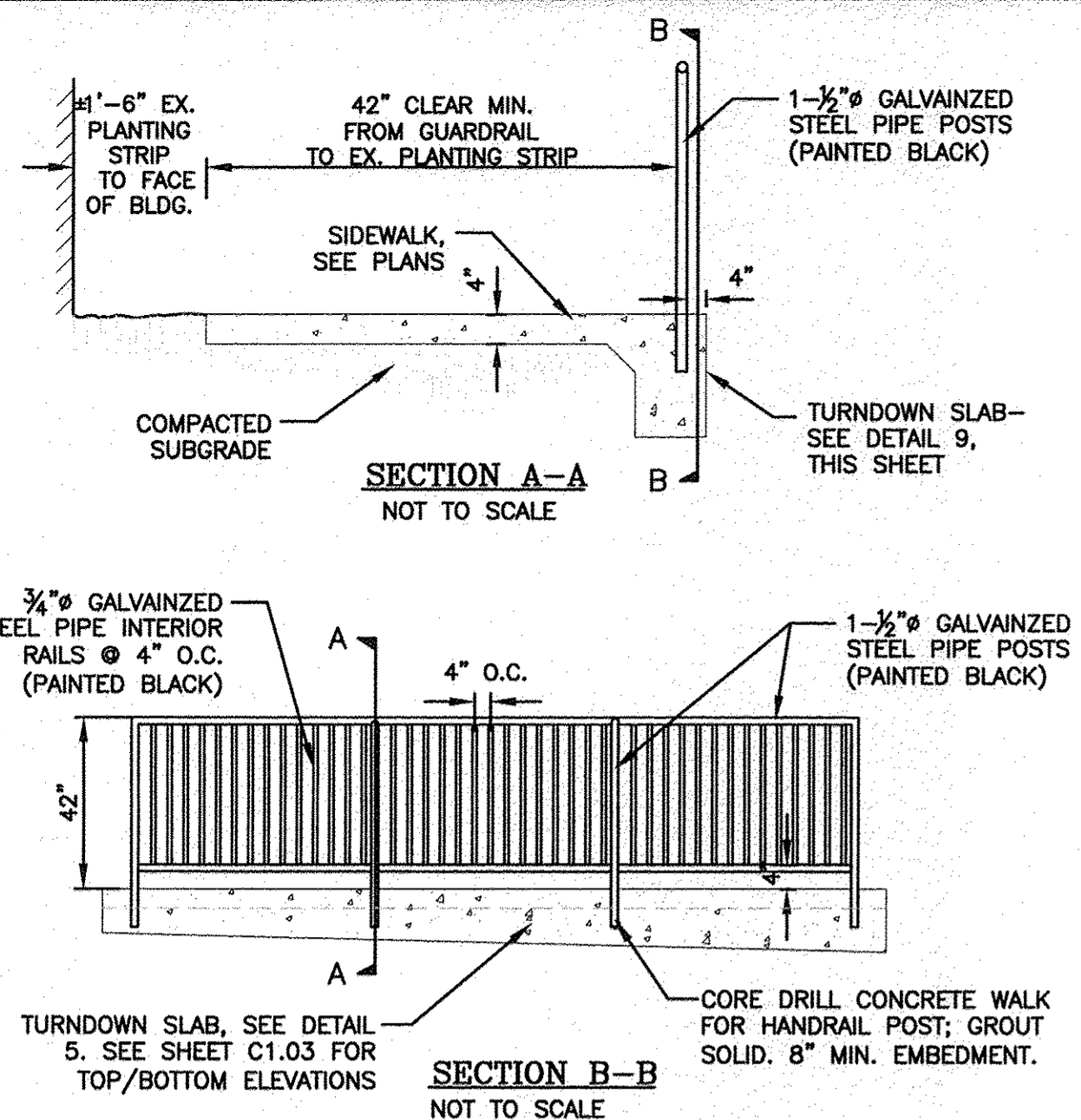
PROTOTYPE LAYOUT

CAD File Name

Description
TRAFFIC CONTROL NOTES & DETAILS

Scale

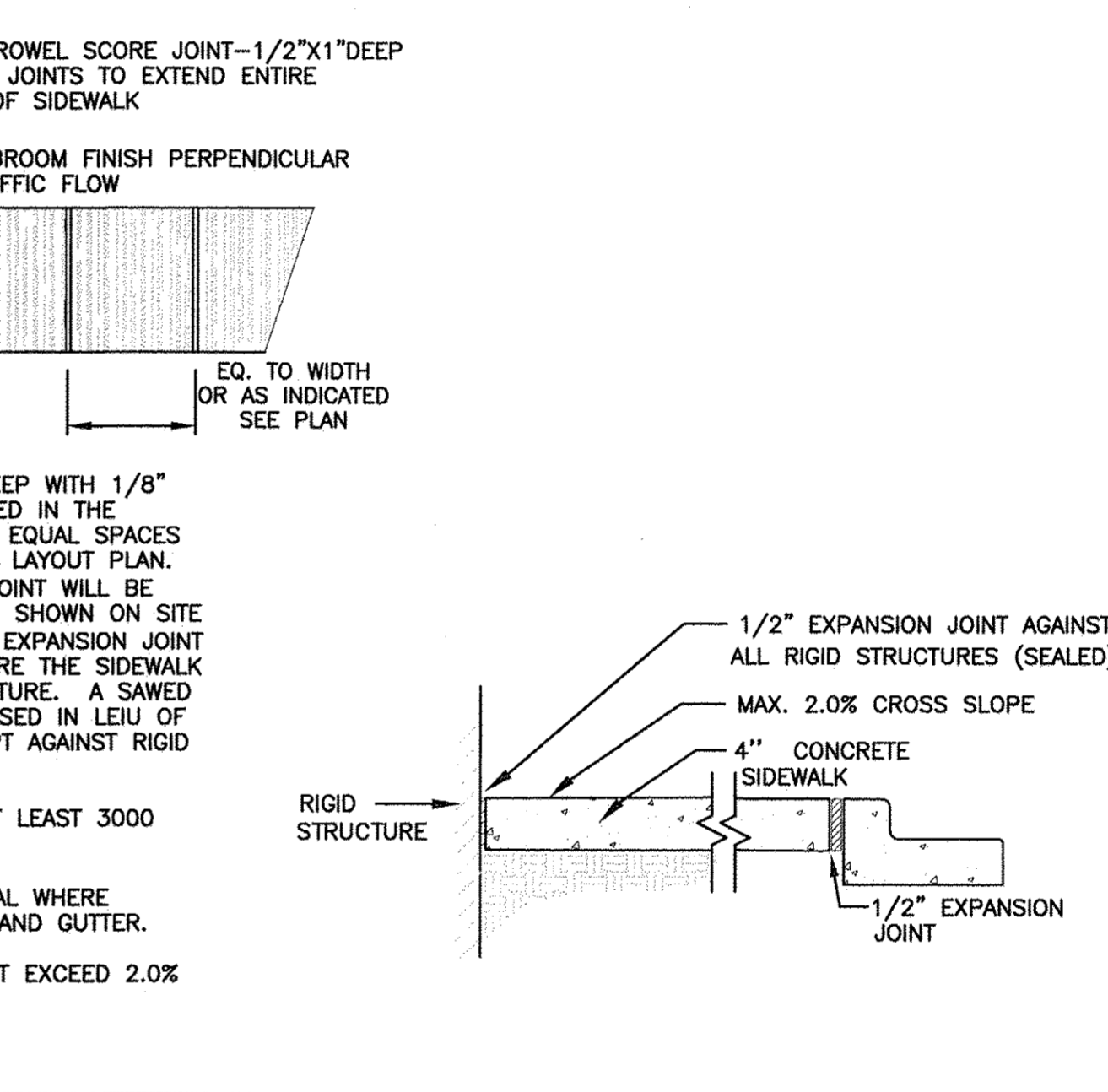
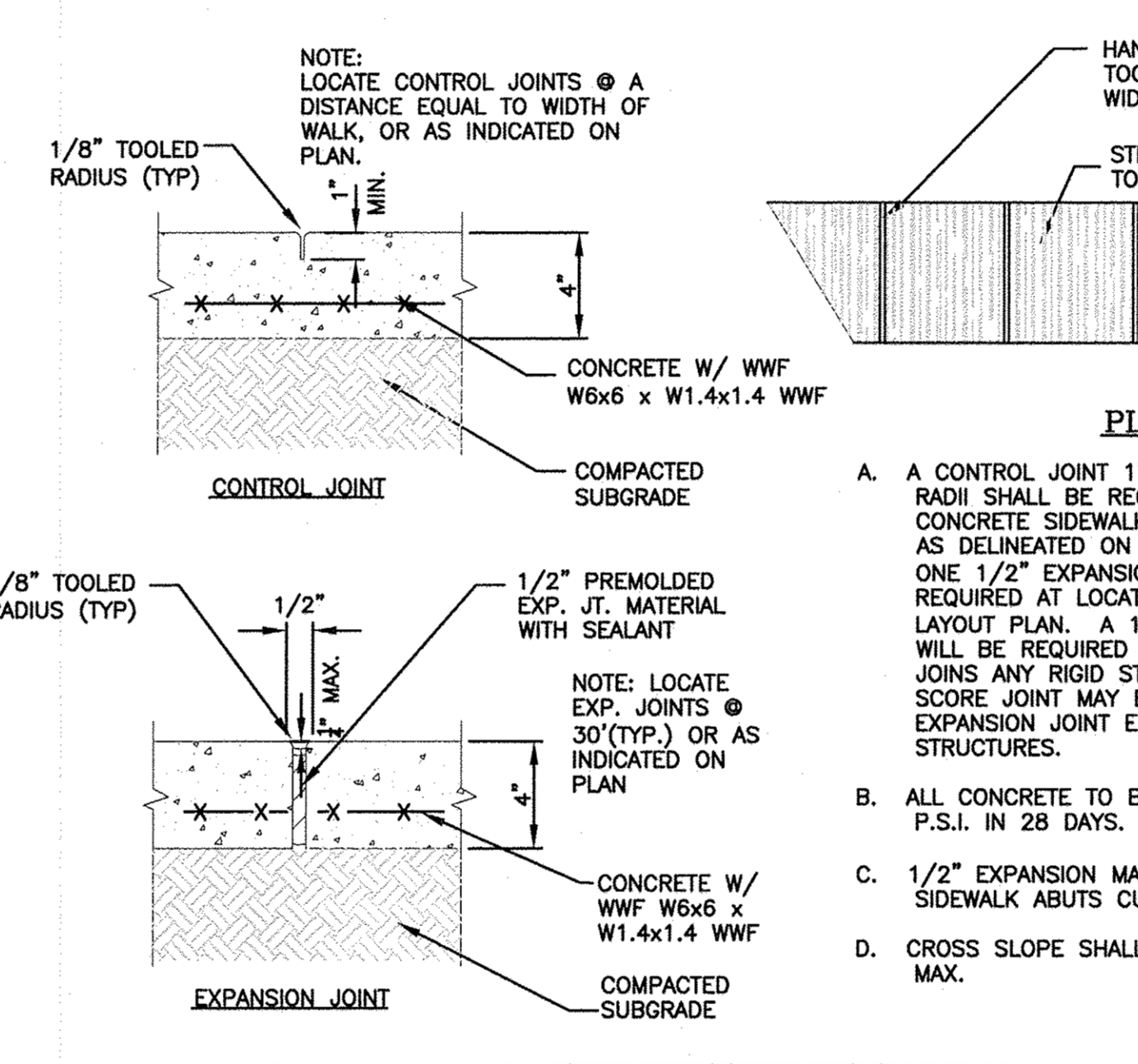
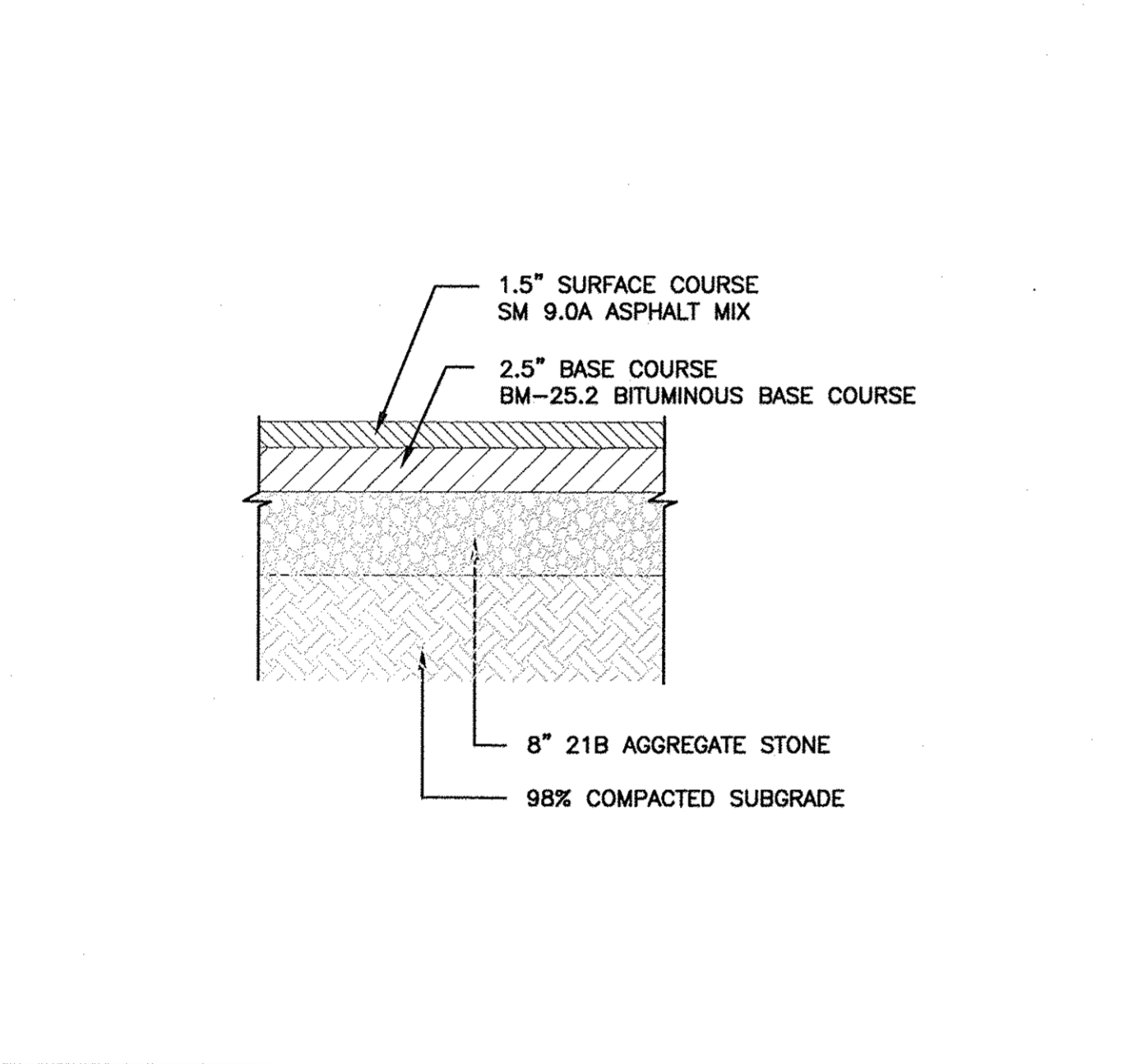
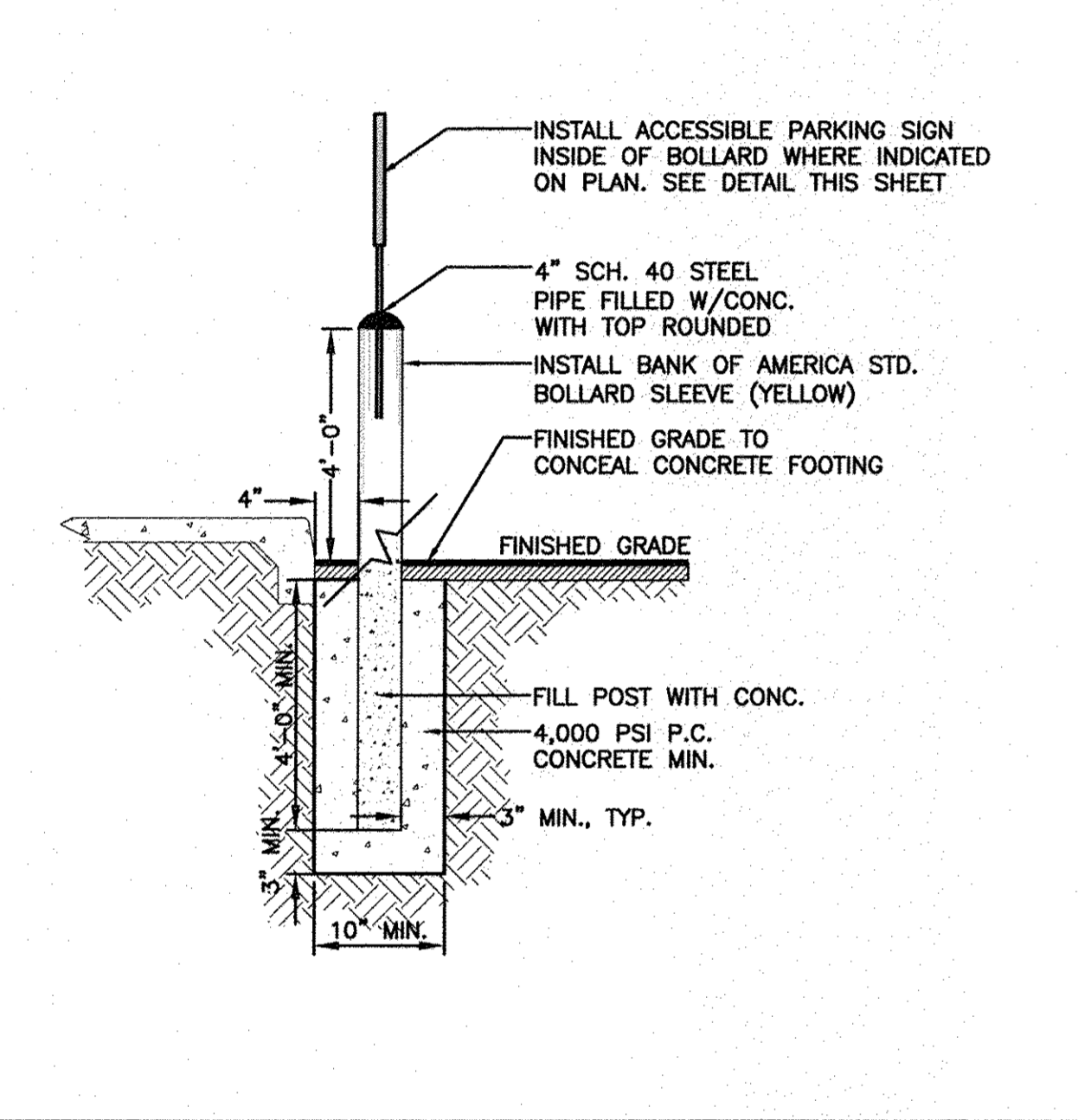
C1.06



10 GUARDRAIL
NOT TO SCALE

9 TURNDOWN SLAB SECTION
NOT TO SCALE

8 ACCESSIBLE STRIPING & SIGNAGE
NOT TO SCALE



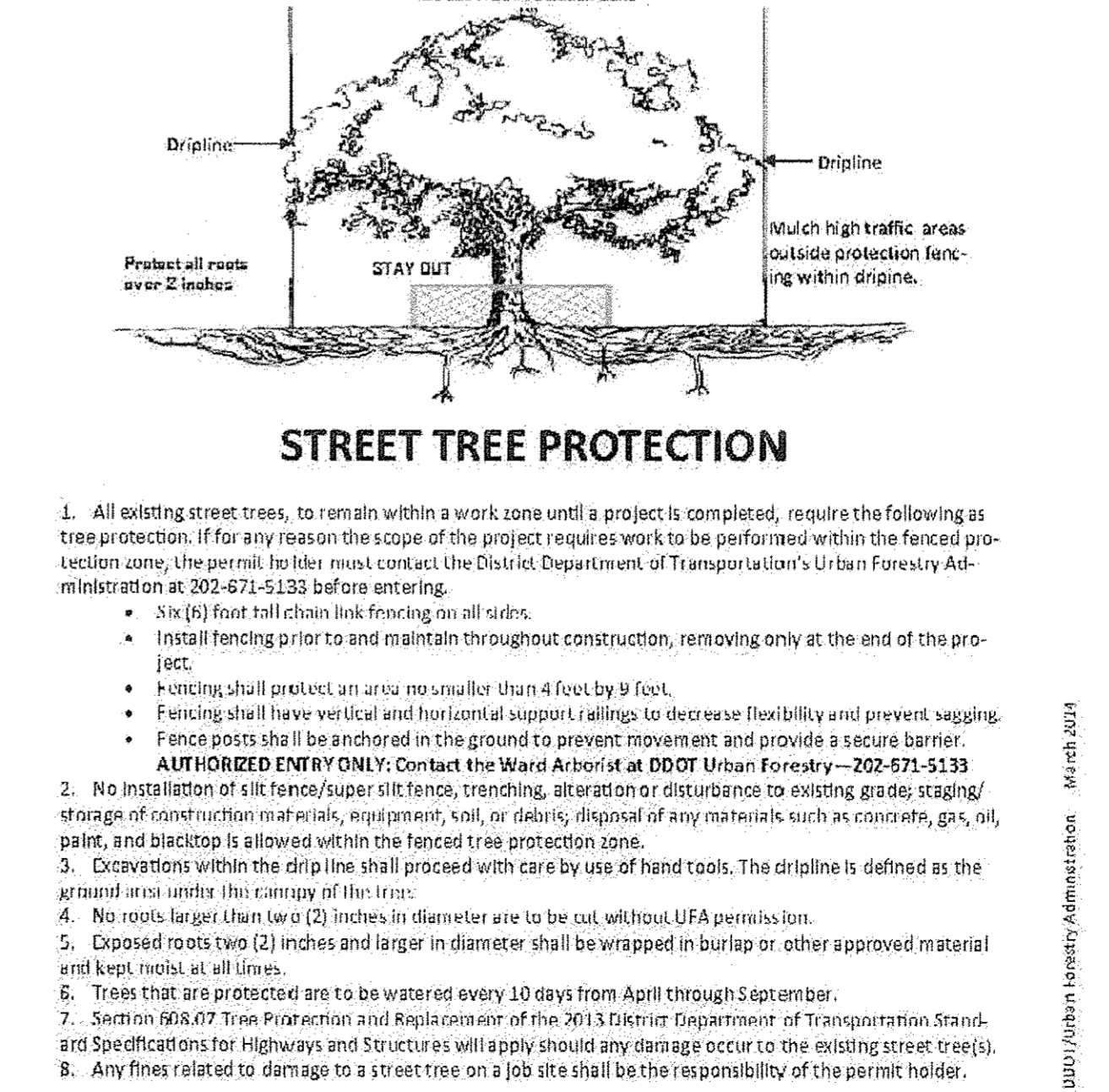
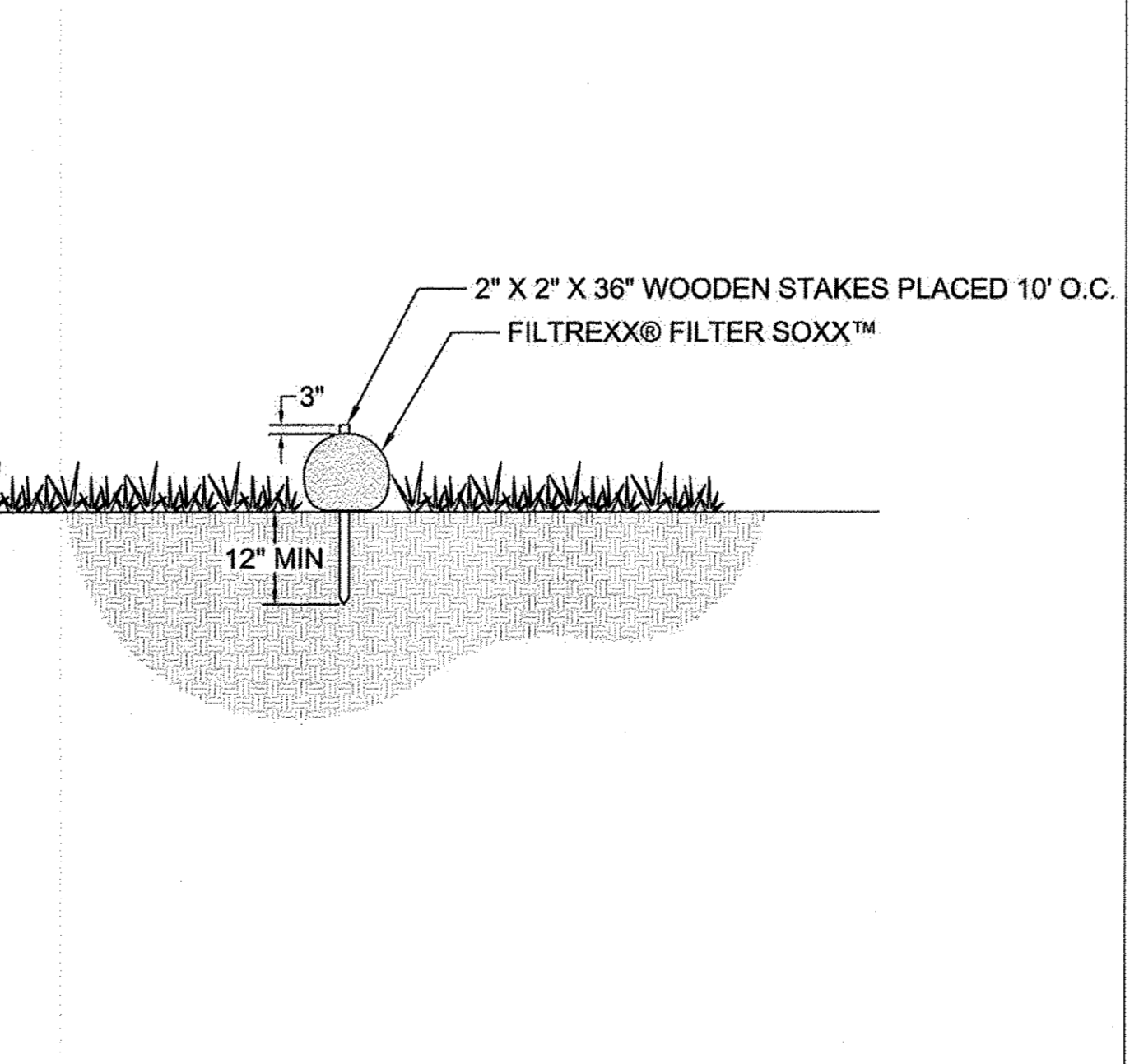
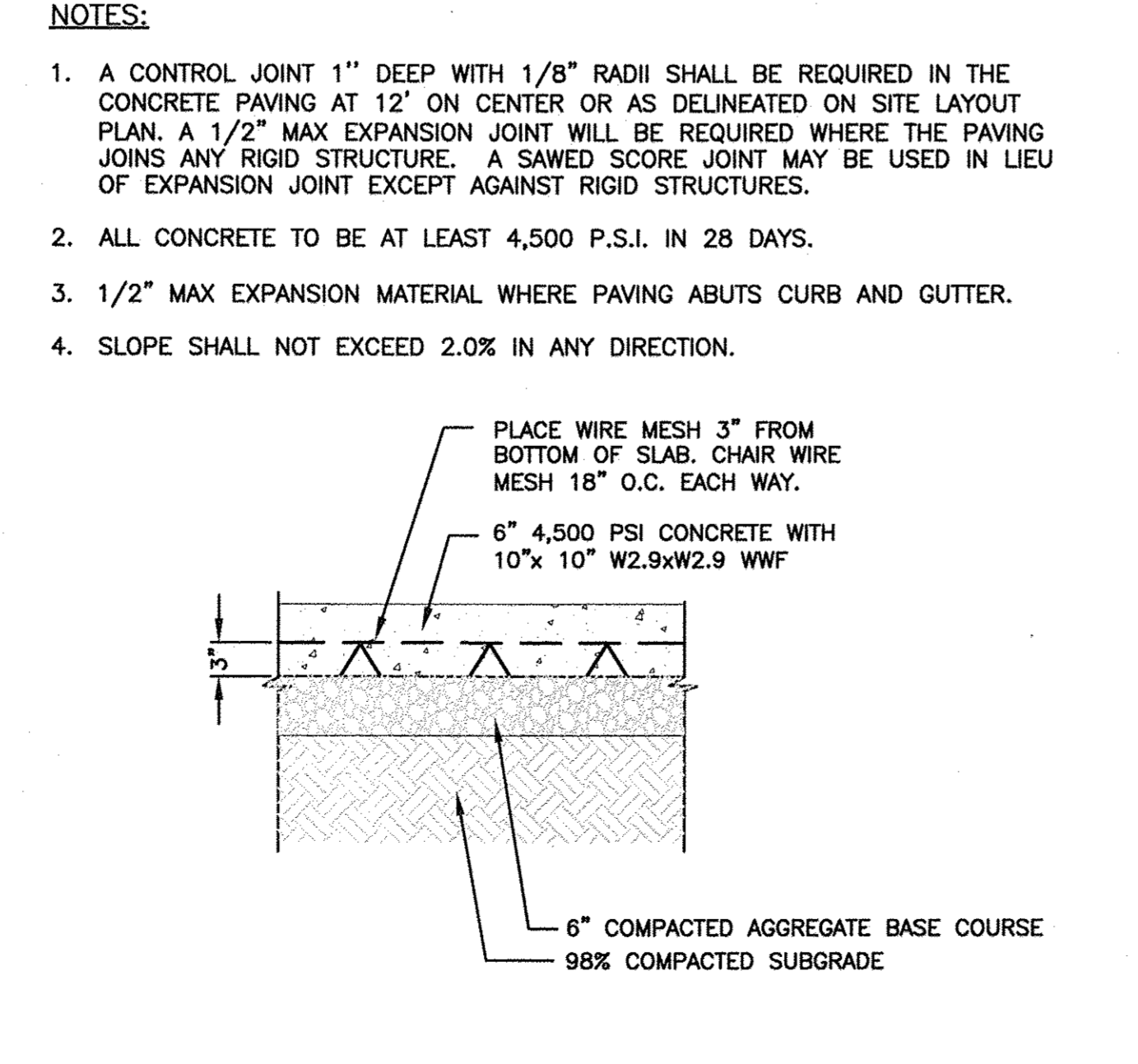
7 PIPE BOLLARD
NOT TO SCALE

6 ASPHALT
NOT TO SCALE

5 CONCRETE SIDEWALK
NOT TO SCALE

NOTES:

- UNLESS NOTED OTHERWISE, ALL CONCRETE SHALL ATTAIN A MINIMUM COMPRESSIVE STRENGTH OF 3000 PSI AT 28 DAYS.
- AGGREGATES SHALL BE NATURAL SAND AND ROCK CONFORMING TO ASTM C33.
- CEMENT SHALL BE PORTLAND CEMENT CONFORMING TO ASTM C-150, TYPE I OR II, LOW ALKALI OR AS REQUIRED TO SATISFY SITE SOIL CONDITIONS AS DETERMINED BY THE PROJECT SOILS ENGINEER.
- WATER SHALL BE POTABLE, CLEAN AND FREE FROM INJURIOUS AMOUNTS OF OIL, ACIDS, ALKALIS, SALTS, ORGANIC MATERIALS, OR OTHER SUBSTANCES THAT MAY BE DELETERIOUS TO CONCRETE OR REINFORCEMENT.
- ADMIXTURES TO BE USED IN CONCRETE SHALL BE SUBJECT TO PRIOR APPROVAL BY THE ENGINEER.
- MAXIMUM PERMISSIBLE WATER-CEMENT RATIOS FOR CONCRETE WHEN STRENGTH DATA FROM TRIAL BATCHES ARE NOT AVAILABLE SHALL CONFORM TO TABLE 4.5 OF ACI STANDARD 318-77 OR BASED ON SULFATE CONTENT OF THE SITE SOILS, AS DETERMINED BY THE SOILS ENGINEER. THE LOWER WATER-CEMENT RATIO SHALL GOVERN.
- CONCRETE SHALL BE CURED WHILE IN A MOIST CONDITION FOR AT LEAST THE FIRST 7 DAYS AFTER PLACEMENT. METHODS FOR ACCELERATED CURING SHALL HAVE PRIOR APPROVAL OF THE ENGINEER.
- THE FOLLOWING MINIMUM CLEAR DISTANCES BETWEEN REINFORCING STEEL AND FACE OF CONCRETE SHALL BE MAINTAINED UNLESS NOTED OTHERWISE:
SLABS ON GRADE, FORMED.....CENTER OF SLAB
CONCRETE BELOW GRADE, FORMED.....2"
CONCRETE BELOW GRADE, UNFORMED.....3"
CONCRETE EXPOSED TO WEATHER.....1-1/2"



4 CONCRETE NOTES
NOT TO SCALE

3 HEAVY-DUTY CONCRETE
NOT TO SCALE

2 SILT SOXX
NOT TO SCALE

1 TREE PROTECTION
NOT TO SCALE

Bank of America Rhode Island Ave. ADA Renovation

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:

LITTLE
DIVERSIFIED ARCHITECTURAL CONSULTING

405 North Fairfax Drive, Suite 650, Arlington, VA 22204
T: 703.908.4501 F: 703.908.4502

www.littleonline.com

This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

© Little 2015

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B1411211
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature: *M. H. Abraham*
DISTRICT OF COLUMBIA
No. PE 907181
PROFESSIONAL ENGINEER
01/22/2015

Project Name: 20150122-00526
Bank of America - Rhode Island Ave. ADA
223-2844-00
PROTOTYPE LAYOUT

CAD File Name:
Description:
SITE DETAILS
Scale:

C2.00

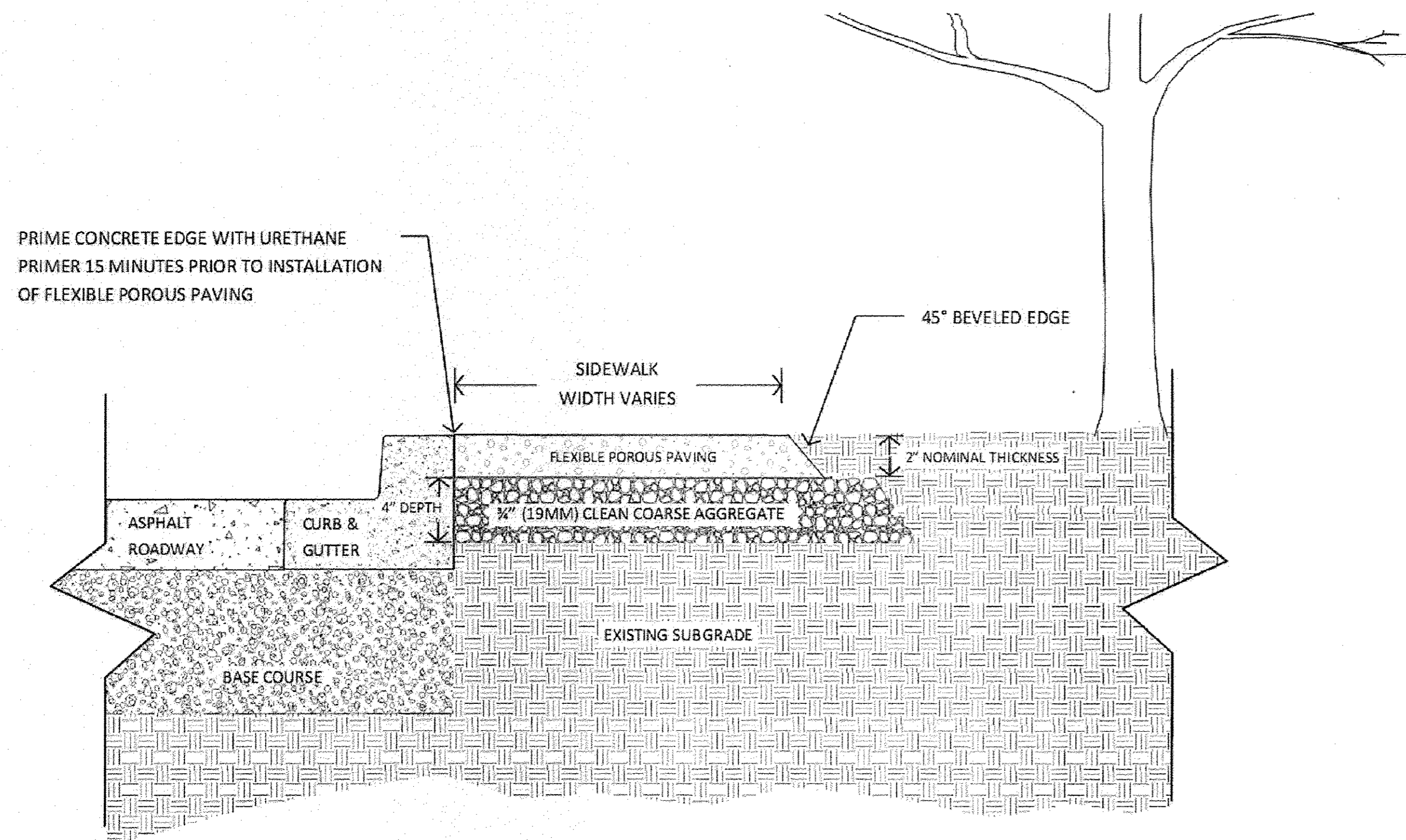
FLEXI-PAVE SIDEWALK MATERIAL

COLOR: MOSSY SLATE
PRODUCT: HD2000

FOR FULL SPEC, PRICING AND INSTALLATION INFORMATION CONTACT:
CAPITOL FLEXI-PAVE, LLC
CONTACT: NOBEL ATKINS
P. (202) 760-1099
37 PIGEON HILL DR
SUITE 278
STERLING, VA 20165

NOTE:

COORDINATE WITH WARD 5 ARBORIST TO ENSURE STREET TREE HEALTH DURING EXISTING SIDEWALK DEMOLITION AND INSTALLATION.
SHAUN MCKIM (DDOT)
SHAUN.MCKIM@DC.GOV



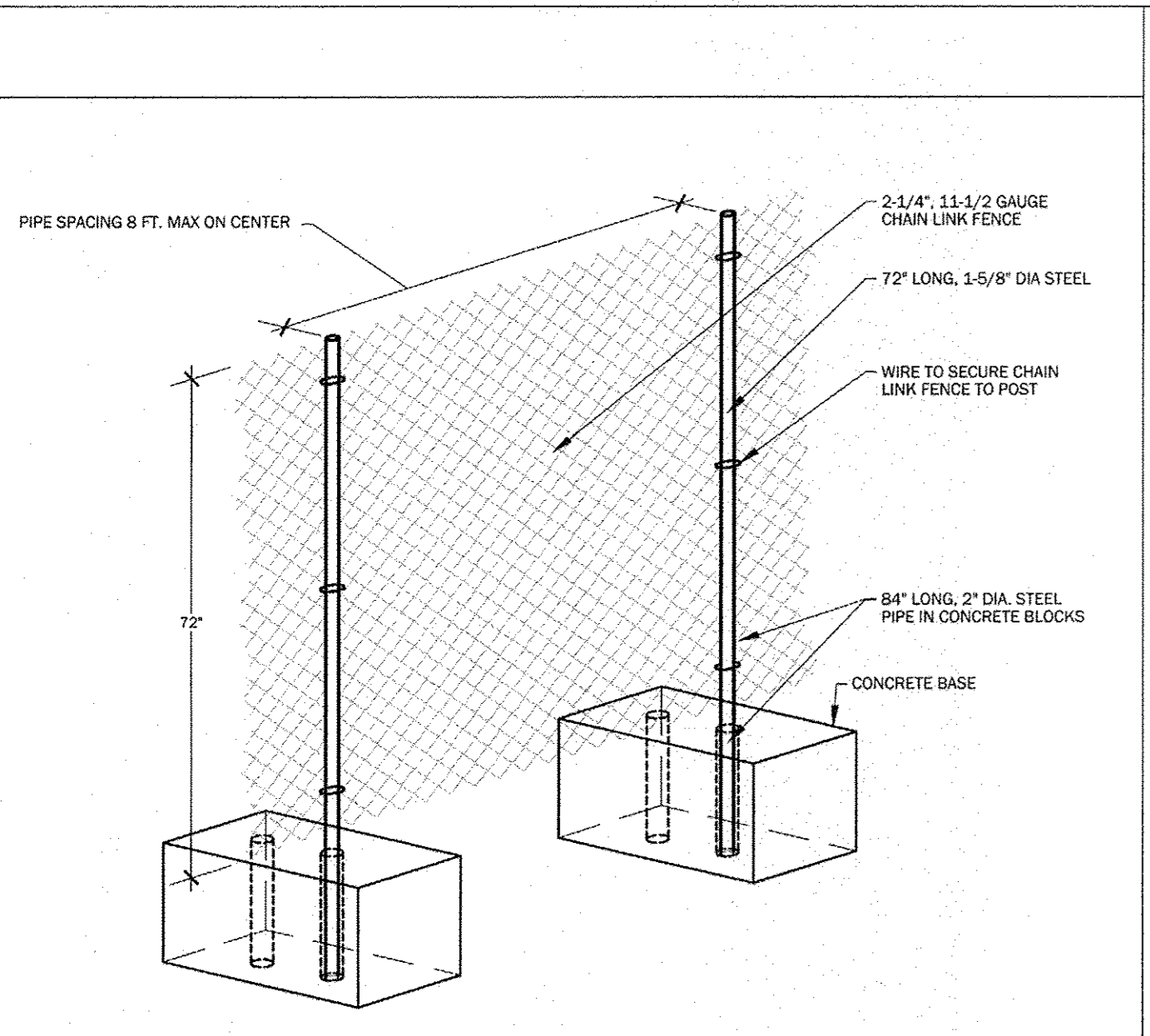
NOTE:
ALL INFORMATION SHOWN IS SUGGESTED. DUE TO APPLICATION VARIANCES, IT IS THE RESPONSIBILITY OF THE ENGINEER OF RECORD TO PROVIDE ALL INFORMATION REQUIRED TO SUIT LOCAL BUILDING CODES AND REGULATIONS. THIS DETAIL IS FOR REPRESENTATIVE PURPOSES ONLY AND SHOULD NOT BE SOLELY USED FOR CONSTRUCTION PURPOSES UNLESS IT HAS BEEN CERTIFIED AND SEALED BY A QUALIFIED ENGINEER.

Capitol Flexi-Pave, LLC
37 Pigeon Hill Dr, Suite 278
Sterling, VA 20165
202.760.1099 PH
571.312.9208 FX
www.capitolflexipave.com

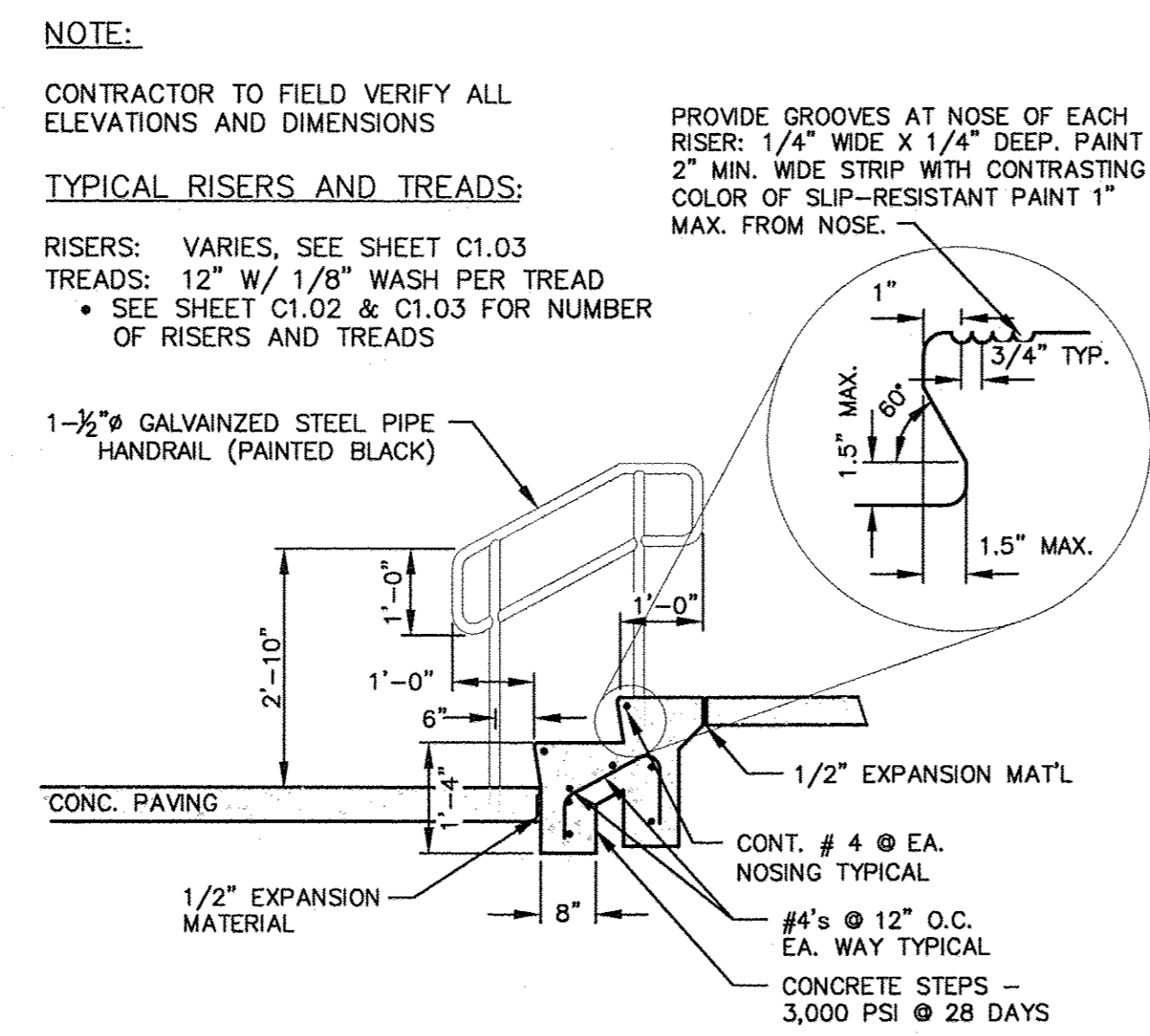
TITLE:
KBI FLEXI™-PAVE SIDEWALK
DRAWN BY: NJA
REVISION DATE: 10/21/13
DATE: 4/2/13
SCALE: NTS

DETAIL NO. 118.00.00

5 FLEXI-PAVE
NOT TO SCALE

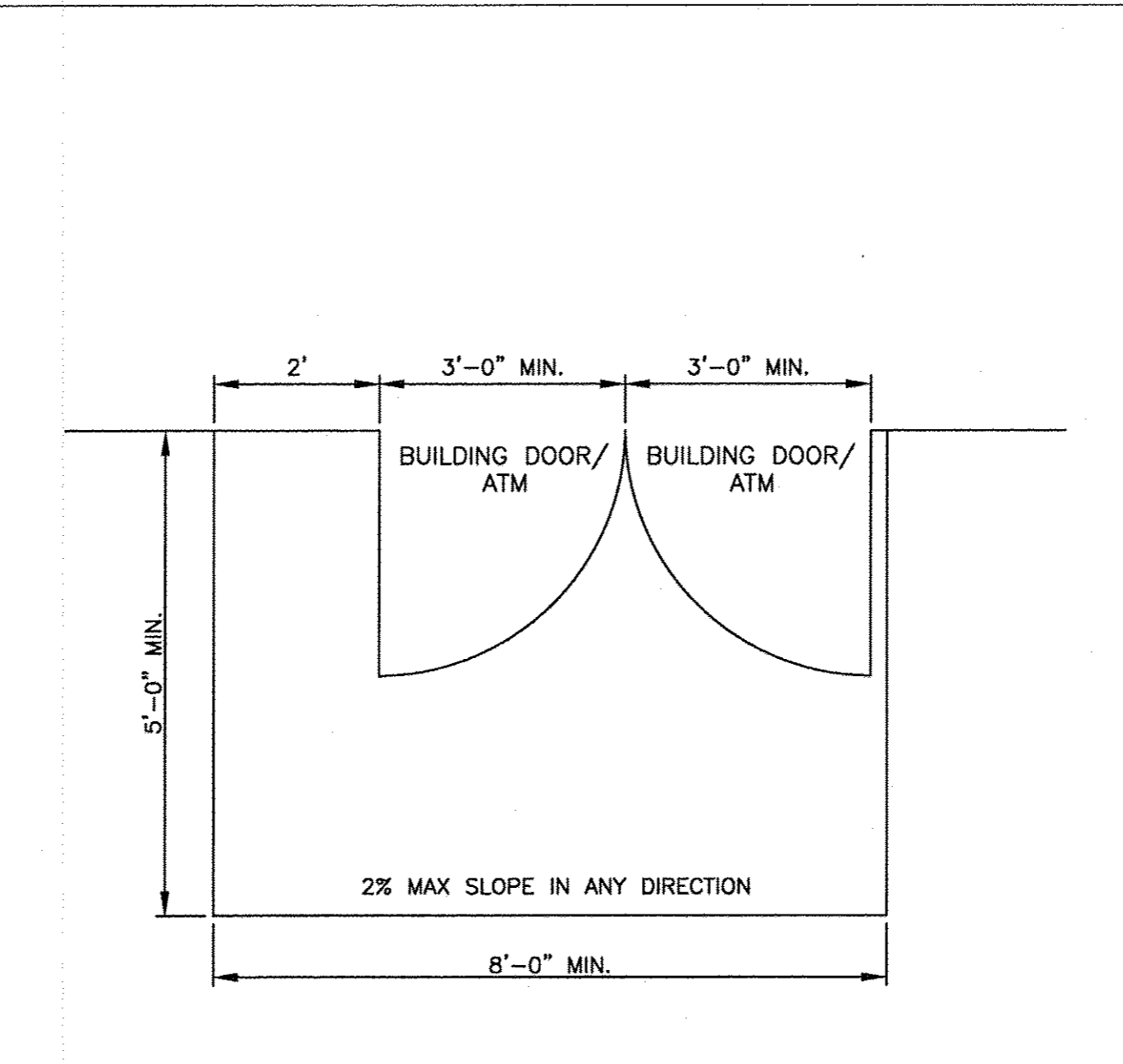


8 SAFETY FENCE
NOT TO SCALE

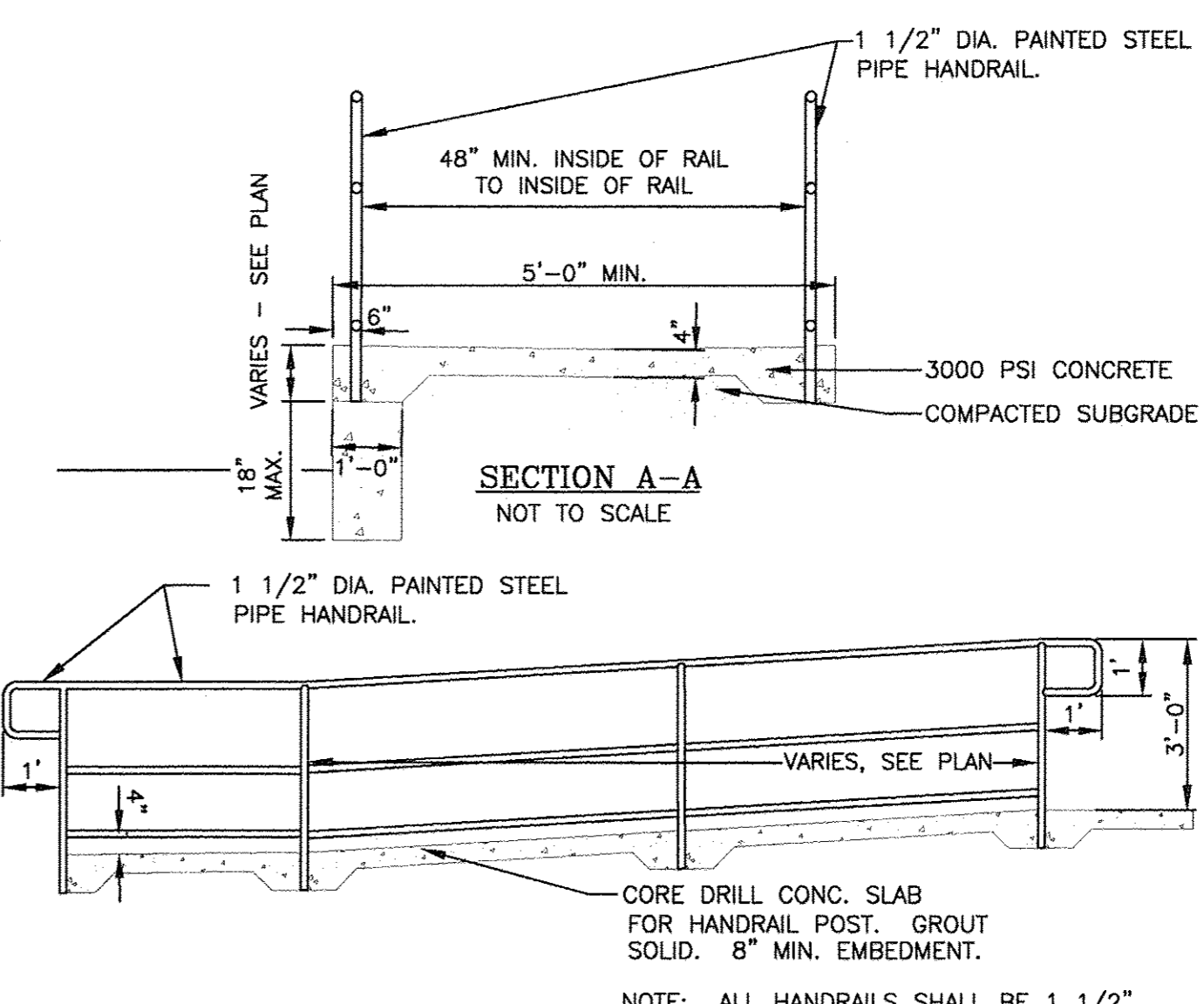


3 CONCRETE STEPS WITH HANDRAIL
NOT TO SCALE

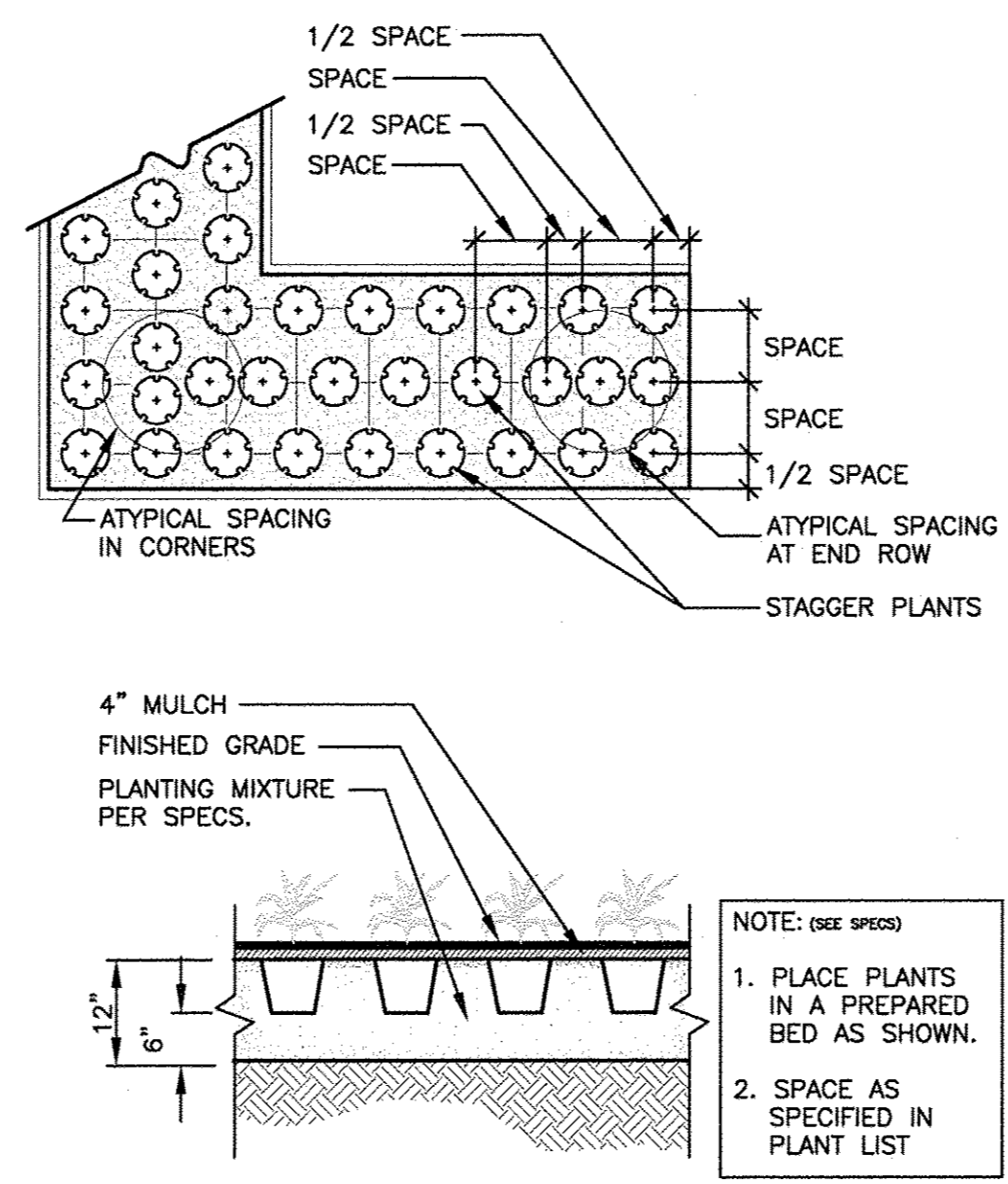
4 LANDSCAPE SCHEDULE
NOT TO SCALE



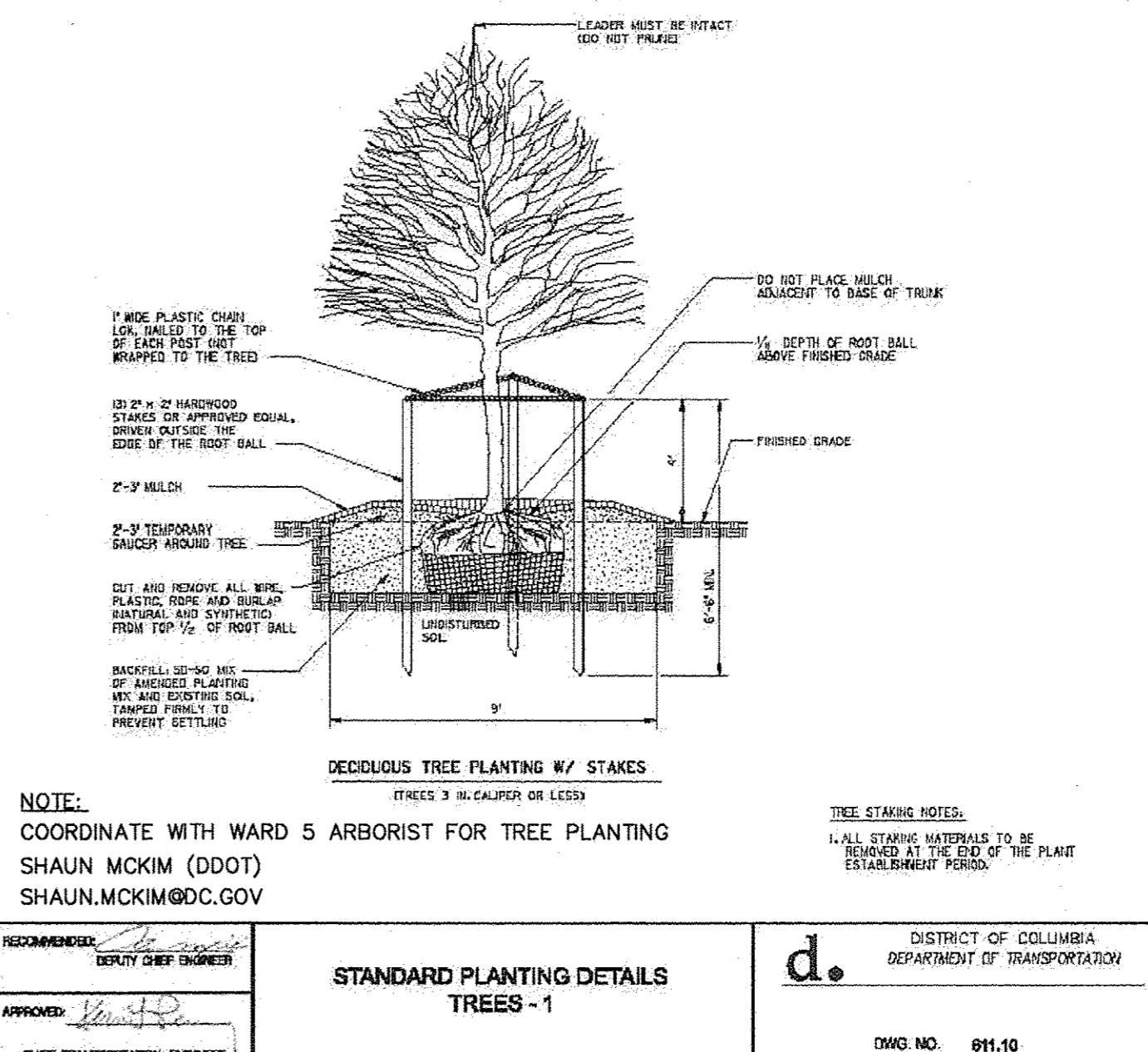
2 ACCESSIBLE EGRESS LANDING
NOT TO SCALE



1 CONCRETE RAMP WITH HANDRAIL
NOT TO SCALE



7 PLANT MASSING & GROUNDCOVER
NOT TO SCALE



6 TREE PLANTING DETAIL
NOT TO SCALE

PLANT LIST

Symbol	Qty.	Label	Botanical Name	Common Name	Size	Spacing	Condition	Description
Deciduous Trees								
	1	TD	<i>Taxodium distichum</i>	Baldcypress	2" caliper/ 10' tall	As Shown	B&B	Specimen See Detail 6/C2.01
Perennials								
	25	CM	<i>Carex morrowii</i>	Japanese Sedge	1 qt.	15" o.c.	Container	See Detail 7/C2.01
	30	LM	<i>Liriope muscari</i>	Lilyturf	1 qt.	15" o.c.	Container	See Detail 7/C2.01
Grasses & Groundcover								
	50	PT	<i>Pachysandra terminalis</i>	Japanese Spurge	1 qt.	12" o.c.	Container	See Detail 7/C2.01

**Bank of America
Rhode Island Ave.
ADA Renovation**

915 Rhode Island Avenue NE
Washington, D.C.

SERIAL NUMBER: DCRHOI-000
NRSP VERSION:
BULLETIN:



This drawing and the design shown are the property of Little Diversified Architectural Consulting. The reproduction, copying or other use of this drawing without their written consent is prohibited and any infringement will be subject to legal action.

Issue	Date & Issue Description	By	Check
01	08/06/14 PERMIT SUBMISSION		
02	01/23/15 DC PUBLIC SPACE COMMENT REVISIONS		

DCRA PERMIT - #B141121
PUBLIC SPACE CONSTRUCTION PERMIT - PA#98997

Seal/Signature:

Project Name: 20150122-00527
Bank of America - Rhode Island Ave. ADA
223-2844-00
PROTOTYPE LAYOUT

CAD File Name
Description
SITE DETAILS
Scale

C2.01